

**PB# 06-32**

**Vails Gate Business Park**

**69-4-3**

TOWN OF NEW WINDSOR  
PLANNING BOARD  
APPROVED COPY  
DATE: August 23, 2007

06-32 Vail's Gate Business Park  
(Formerly Tarkett) Rt. 94 (DePuy)

**JOHN COLLINS  
ENGINEERS, P.C.**

TRAFFIC • TRANSPORTATION ENGINEERS

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**TRAFFIC IMPACT STUDY**

\*\*\*\*\*

**TARKETT PROPERTY REDEVELOPMENT**

**NYS ROUTE 94 AND TARKETT DRIVE**

**TOWNS OF CORNWALL AND NEW WINDSOR, NEW YORK**

**JOB NO. 309**

**AUGUST 2, 2006**

**REVISED DECEMBER 19, 2006**

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SECTION I  
INTRODUCTION

A. PROJECT DESCRIPTION AND LOCATION (Figure No. 1)

This study has been prepared to evaluate the potential traffic impacts of the redevelopment of the former Tarkett property. The property is located south of NYS Route 94 on Tarkett Drive in the Towns of Cornwall and New Windsor, New York. The Project calls for the three existing buildings, totaling 256,892 sq ft, to be renovated for general light manufacturing/warehouse space reoccupancy. The site location is identified on Figure No. 1.

A design year of 2009 has been utilized in this report to evaluate future traffic conditions.

B. SCOPE OF STUDY

This study has been prepared to evaluate the existing and future traffic conditions at the intersection of NYS Route 94 and Tarkett Drive. In the course of completing this study, data regarding existing roadway and traffic conditions were collected by representatives of John Collins Engineers, P.C. In addition, historical data including data from the NYSDOT were also referenced. Detailed turning movement traffic counts were conducted at the intersection to establish the existing traffic volumes for the weekday peak hours. The existing traffic volumes were then projected to a future design year utilizing a background growth factor of 3% which was developed based on historical information from the NYSDOT and to account for traffic from other planned or proposed developments in the area. These projected traffic volumes represent the design year No-Build Traffic Volumes.

Estimates of the site generated traffic volumes for the proposed development were computed utilizing information published by the Institute of Transportation Engineers (ITE). These volumes were then added to the 2009 No-Build Traffic Volumes to obtain the 2009 Build Traffic Volumes with a completion of the project.

The Existing, No-Build and Build Traffic Volumes were then compared to roadway capacities to determine the existing and future Levels of Service and traffic operating conditions. Based on the results of the analysis, recommendations for improvements were then made.

SECTION II

EXISTING ROADWAY AND TRAFFIC CONDITIONS

A. DESCRIPTION OF EXISTING ROADWAY NETWORK

Detailed field inspections of the roadways in the vicinity of the site were completed. At the time of the surveys, information regarding existing roadway geometrics, type and location of traffic control devices and existing traffic flow characteristics were identified. As mentioned previously, the site will be served by an access connection to NYS Route 94, a description of this and Tarkett Drive follows:

1. NYS Route 94

NYS Route 94 is a state highway which runs generally in a northeast/southwest direction throughout Orange County. The roadway originates at an intersection with NYS Route 9W in the Town of New Windsor and continues in a southwesterly direction. It intersects with NYS Route 32 and NYS Route 300 east of the site. It continues as a three lane roadway passing the site. Immediately west of Tarkett Drive there is a signal controlled railroad crossing located just prior to the NYS Thruway underpass. The roadway continues in a southwesterly direction through other portions of Orange County including the Towns of Blooming Grove, Chester and Goshen. It intersects at an interchange with NYS Route 17 and then continuing south into New Jersey. In the immediate vicinity of the site, the roadway consists of one lane in each direction and has a posted speed limit of 45 mph.

2. Tarkett Drive

Tarkett Drive is an existing roadway that services only the Tarkett property. It intersects with NYS Route 94 at an unsignalized intersection and continues south to the site.

B. 2006 EXISTING TRAFFIC VOLUMES (Figures No. 2 and 3)

Detailed traffic count data for the intersection of NYS Route 94 and Tarkett Drive was collected by conducting turning movement traffic counts. The AM count was performed on Tuesday, June 20, 2006 from 7:00AM – 9:00AM and the PM count was performed on Monday, June 19, 2006 from 3:00PM – 6:00PM.

These counts were also compared to other historical data and were summarized for the weekday AM (7:00-8:00AM) and PM (4:30 – 5:30PM) peak hours to identify current volumes for these intersections. The resulting 2006 Existing Traffic Volumes are shown on Figures No. 2 and 3.

### SECTION III

#### EVALUATION OF FUTURE TRAFFIC CONDITIONS

A. 2009 NO-BUILD TRAFFIC VOLUMES (Figures No. 4 and 5)

The 2006 Existing Traffic Volumes shown on Figures No. 2 and 3 were projected to a future 2009 design year, by utilizing a background growth factor of 3% per year. This growth factor was developed based on historical data and was used to also account for other potential development traffic in the area. The resulting 2009 No-Build Traffic volumes for the weekday AM and PM peak hours are shown on Figures No. 4 and 5.

B. SITE-GENERATED TRAFFIC VOLUMES (Table No. 1)

In order to determine the traffic which will be generated in association with the redevelopment of the site, information published by the Institute of Transportation engineers (ITE) as contained in their report entitled Trip Generation: 7<sup>th</sup> Edition, 2006 was referenced. Utilizing this data, the peak-hour trip generation rates and corresponding site-generated traffic volumes were estimated and are shown in Table No. 1. It should be noted that the redevelopment of the site should result in a better distribution of traffic to and from the site since the types of uses should result in less "peaking of traffic" as compared to that experienced at the shift change of the former Tarkett operation.

C. ARRIVAL AND DEPARTURE DISTRIBUTIONS (Figures No. 6 and 7)

An arrival and departure distribution was developed based upon a review of the existing traffic volumes in order to assign the site-generated traffic volumes to the roadway network. The anticipated distributions are shown on Figures No. 6 and 7, respectively.

D. 2009 BUILD TRAFFIC VOLUMES (Figures No. 8, 9, 10 and 11)

Utilizing the arrival and departure distributions, the site-generated traffic volumes were added to the roadway system and site driveways. The site-generated traffic volumes are shown on Figures No. 8 and 9. These site-generated traffic volumes were combined with the design year No-Build Traffic Volumes to obtain the 2009 Build Traffic Volumes, which are shown on Figures No. 10 and 11 for the AM and PM peak hours, respectively.

E. DESCRIPTION OF ANALYSIS PROCEDURES

In order to determine existing and future traffic operating conditions at the study area intersections, it was necessary to perform capacity analyses. The following is a brief description of the analysis method utilized in this report:

The unsignalized intersection capacity analysis method utilized in this report was also performed in accordance with the procedures described in the 2000 Highway Capacity Manual. The procedure is based on total elapsed time from when a vehicle stops at the end of the queue until the vehicle departs from the stop line. The average total delay for any particular critical movement is a function of the service rate or capacity of the approach and the degree of saturation. In order to identify the Level of Service, the average amount of vehicle delay is computed for each critical movement to the intersection.

Additional information concerning signalized and unsignalized Levels of Service can be found in Appendix "D" of this report.

**F. RESULTS OF ANALYSIS AND RECOMMENDATIONS (Table No. 2)**

Utilizing the procedures outlined above, a capacity analysis was conducted at the intersection of NYS Route 94 and Tarkett Drive, utilizing the existing, No-Build and Build Traffic Volumes. The results of the capacity analyses are summarized in Table No. 2. Based on a review of the results of the field inspections and the analysis results, the following recommendations are made.

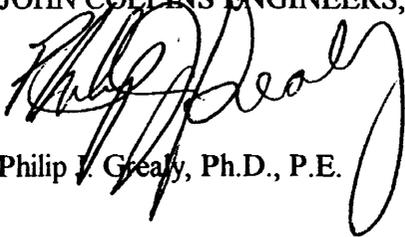
1. Along the south side of Route 94 looking west of Tarkett Drive, the existing vegetation within the Route 94 right-of-way should be pruned to ensure adequate sight lines for exiting vehicles. In addition, if the railroad siding is no longer active, the applicant should pursue a possible “exempt” posting.
2. The warning signing and beacon assembly located on the Route 94 approach west of Tarkett Drive should be upgraded and reactivated.
3. To improve on site access for emergency vehicles, Tarkett Drive should be reviewed with the Town emergency services personnel about potential cross connections of the internal roadways.

G. SUMMARY AND CONCLUSIONS

With the completion of the above improvements, the capacity analysis indicate that the proposed redevelopment of the Tarkett Property will not result in a significant negative traffic impact on the adjacent roadway, and safe and efficient operation will exist.

Respectfully submitted,

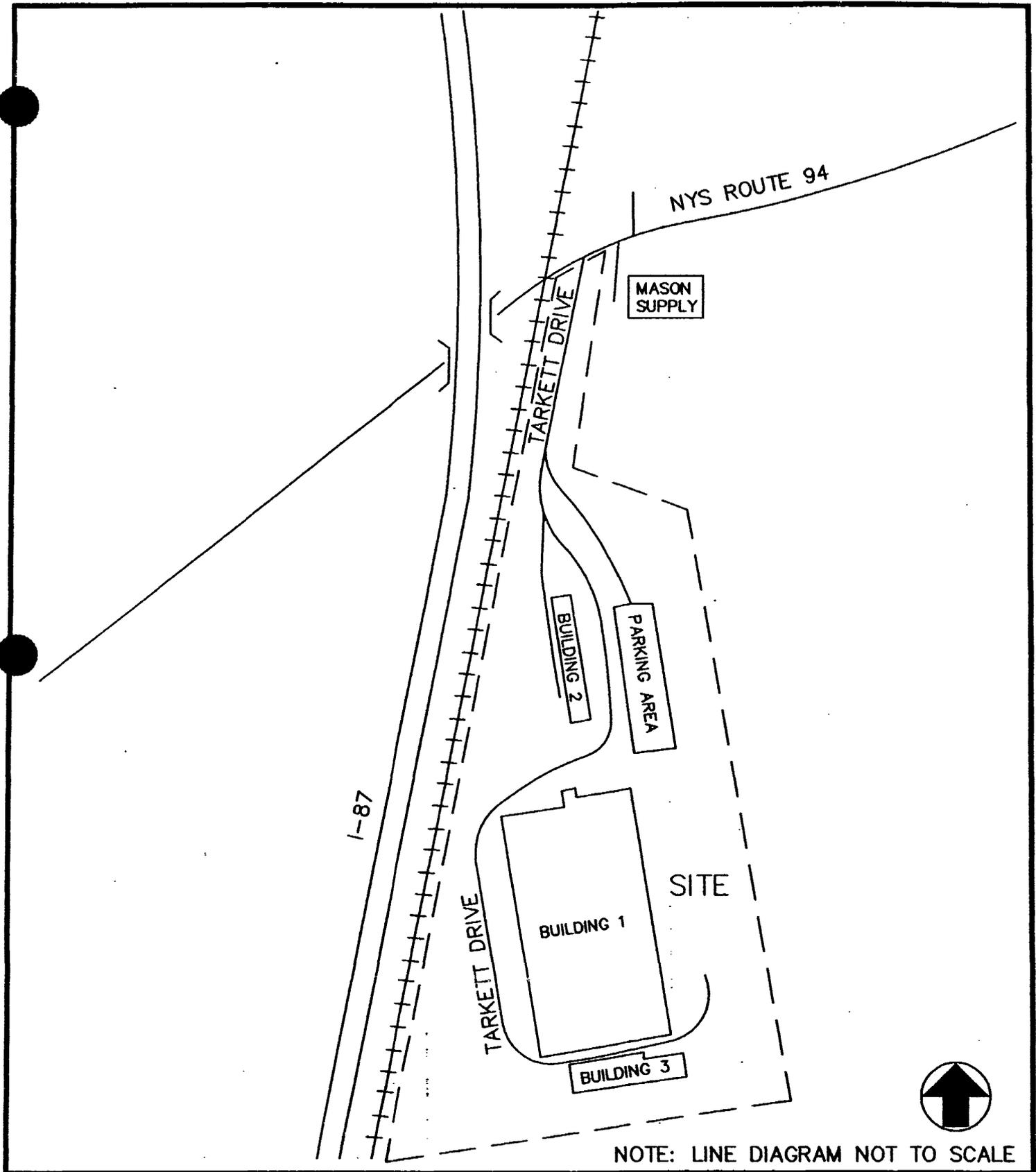
JOHN COLLINS ENGINEERS, P.C.



Philip J. Grealy, Ph.D., P.E.

APPENDIX "A"

FIGURES

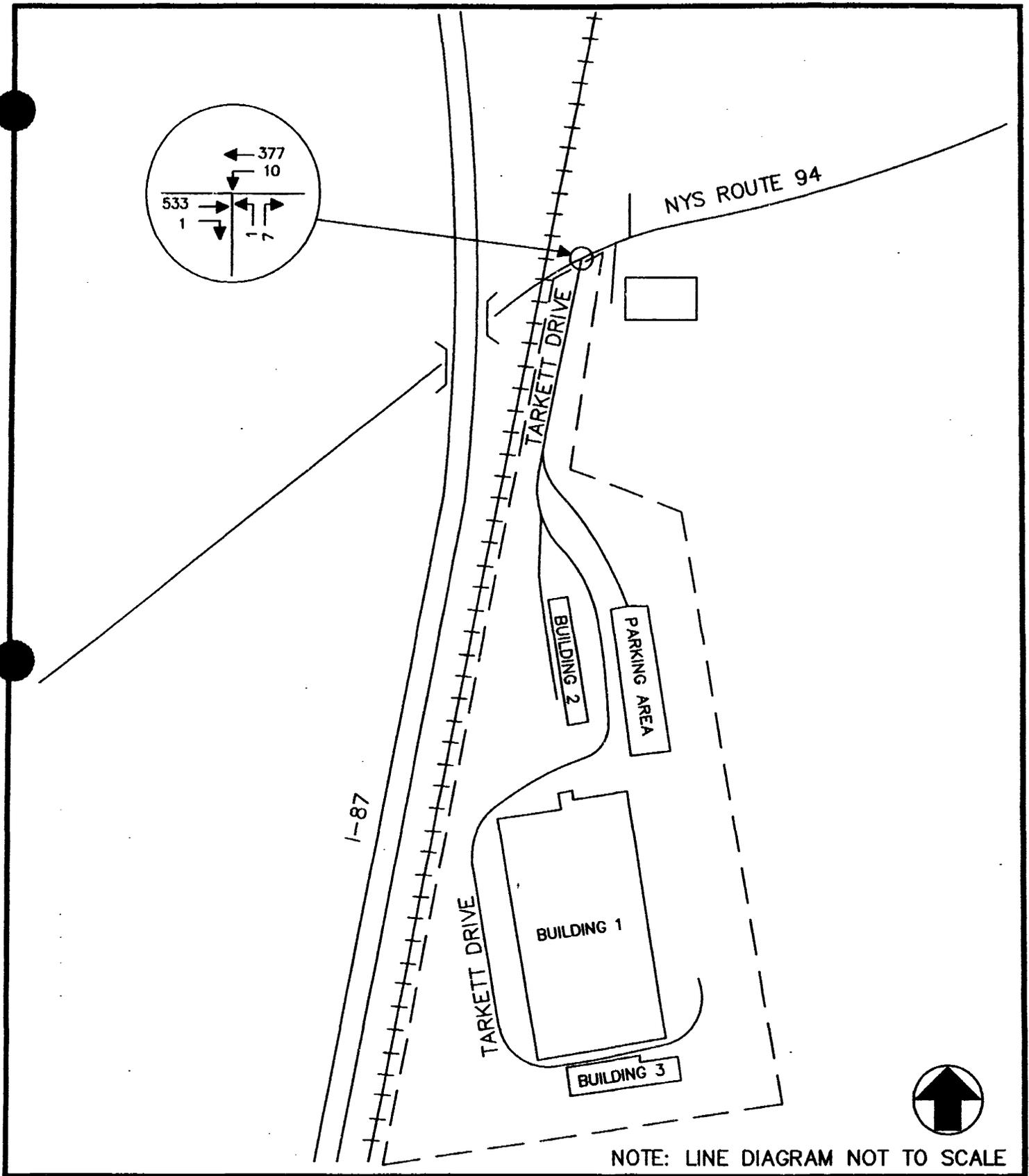


**TARKETT PROPERTY REDEVELOPMENT**  
 TOWNS CORNWALL AND NEW WINDSOR, NEW YORK

**SITE LOCATION MAP**

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PROJECT NO. 309 DATE: JULY, 2006 FIG. NO. 1



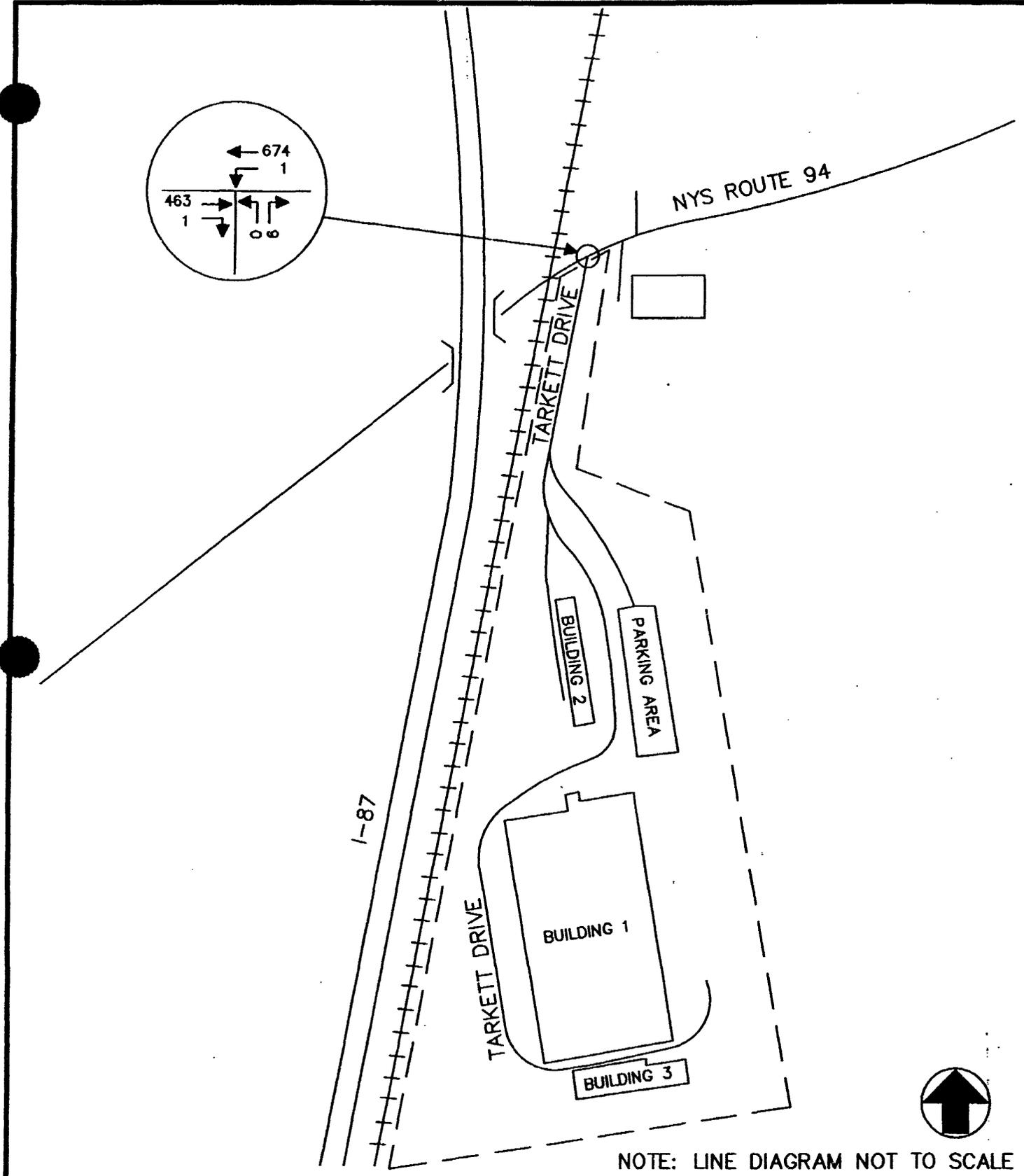
NOTE: LINE DIAGRAM NOT TO SCALE

**TARKETT PROPERTY REDEVELOPMENT**  
 TOWNS CORNWALL AND NEW WINDSOR, NEW YORK

**2006 EXISTING TRAFFIC VOLUMES**  
 WEEKDAY PEAK AM HOUR

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 HAWTHORNE, NEW YORK

PROJECT NO. 309 DATE: JULY, 2006 FIG. NO. 2



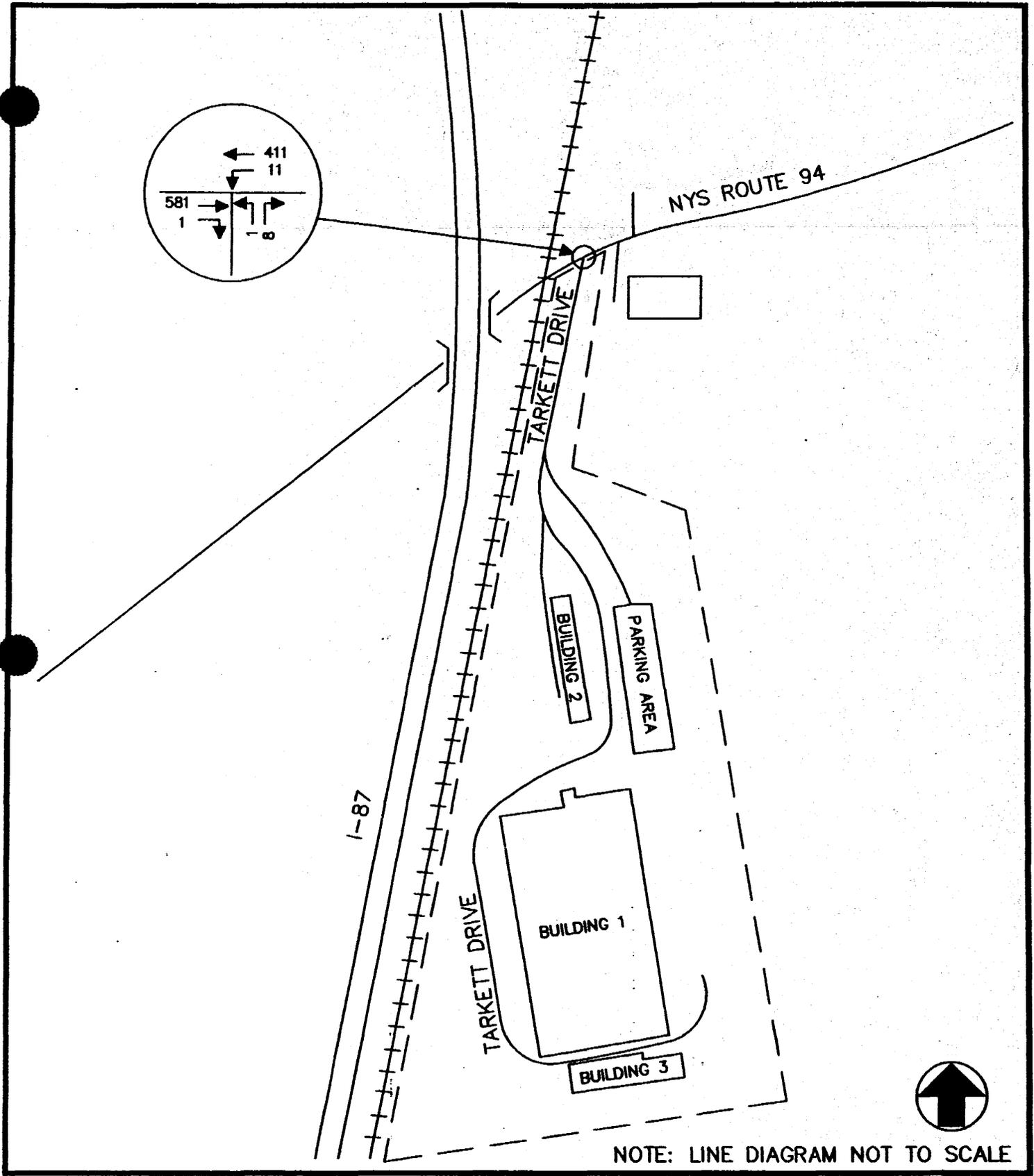
NOTE: LINE DIAGRAM NOT TO SCALE

**TARKETT PROPERTY REDEVELOPMENT**  
 TOWNS CORNWALL AND NEW WINDSOR, NEW YORK

**2006 EXISTING TRAFFIC VOLUMES**  
 WEEKDAY PEAK PM HOUR

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 HAWTHORNE, NEW YORK

PROJECT NO. 309 DATE: JULY, 2006 FIG. NO. 3



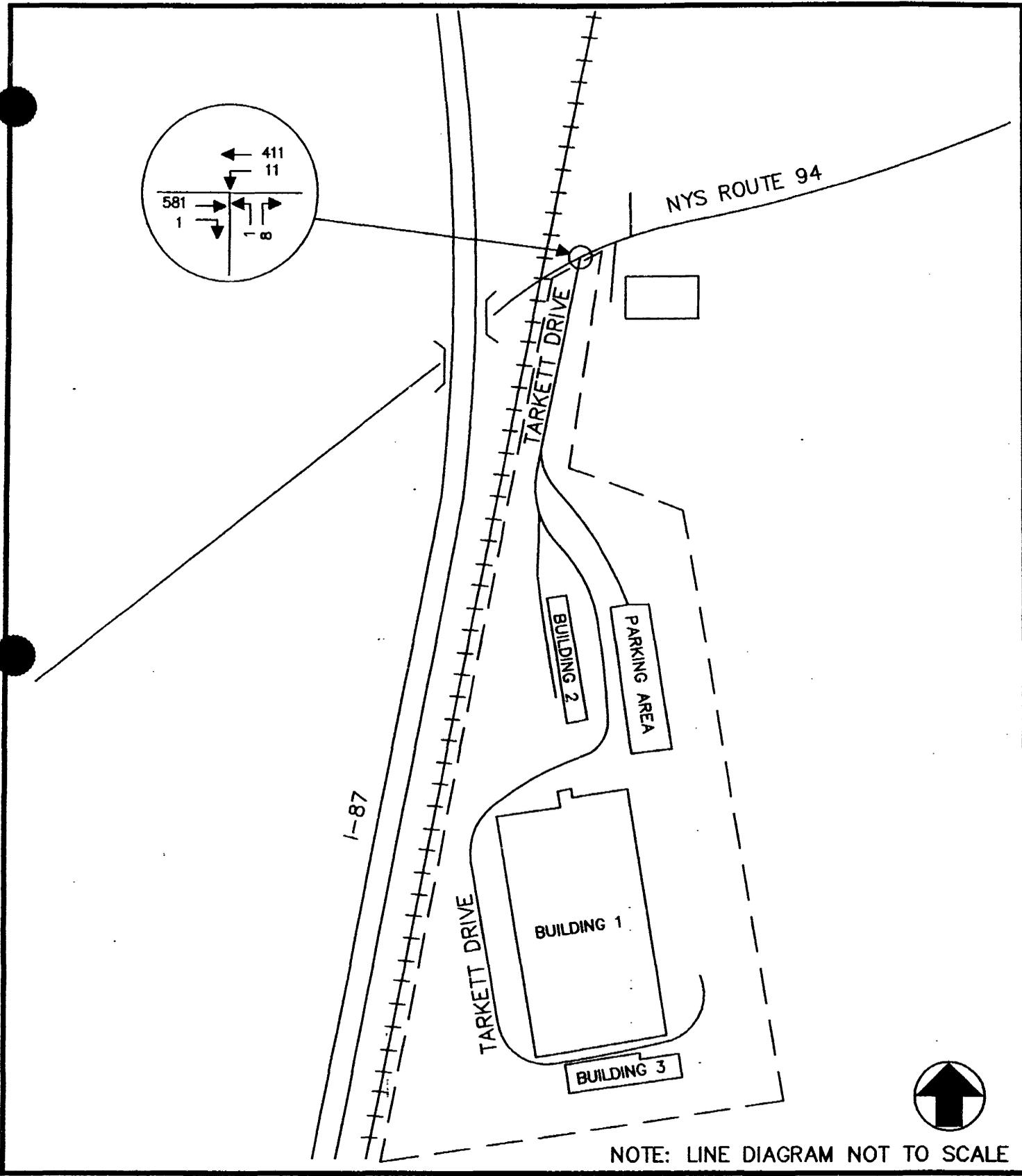
NOTE: LINE DIAGRAM NOT TO SCALE

TARKETT PROPERTY REDEVELOPMENT  
TOWNS CORNWALL AND NEW WINDSOR, NEW YORK

2009 NO-BUILD TRAFFIC VOLUMES  
WEEKDAY PEAK AM HOUR

JOHN COLLINS ENGINEERS, P.C.  
HAWTHORNE, NEW YORK

PROJECT NO. 309 DATE: JULY, 2006 FIG. NO. 4



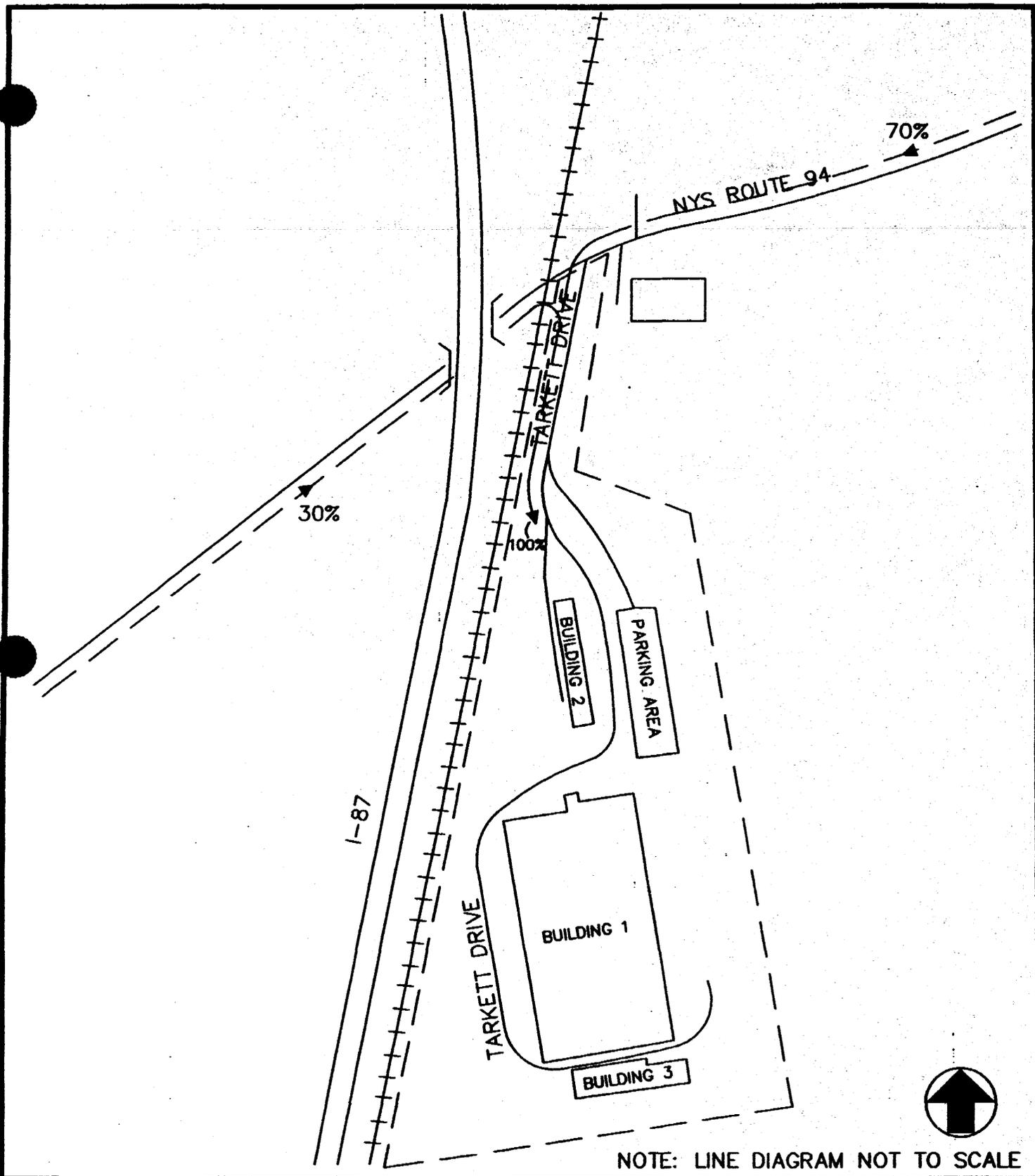
NOTE: LINE DIAGRAM NOT TO SCALE

TARKETT PROPERTY REDEVELOPMENT  
TOWNS CORNWALL AND NEW WINDSOR, NEW YORK

2009 NO-BUILD TRAFFIC VOLUMES  
WEEKDAY PEAK AM HOUR

JOHN COLLINS ENGINEERS, P.C.  
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PROJECT NO. 309 DATE: JULY, 2006 FIG. NO. 4



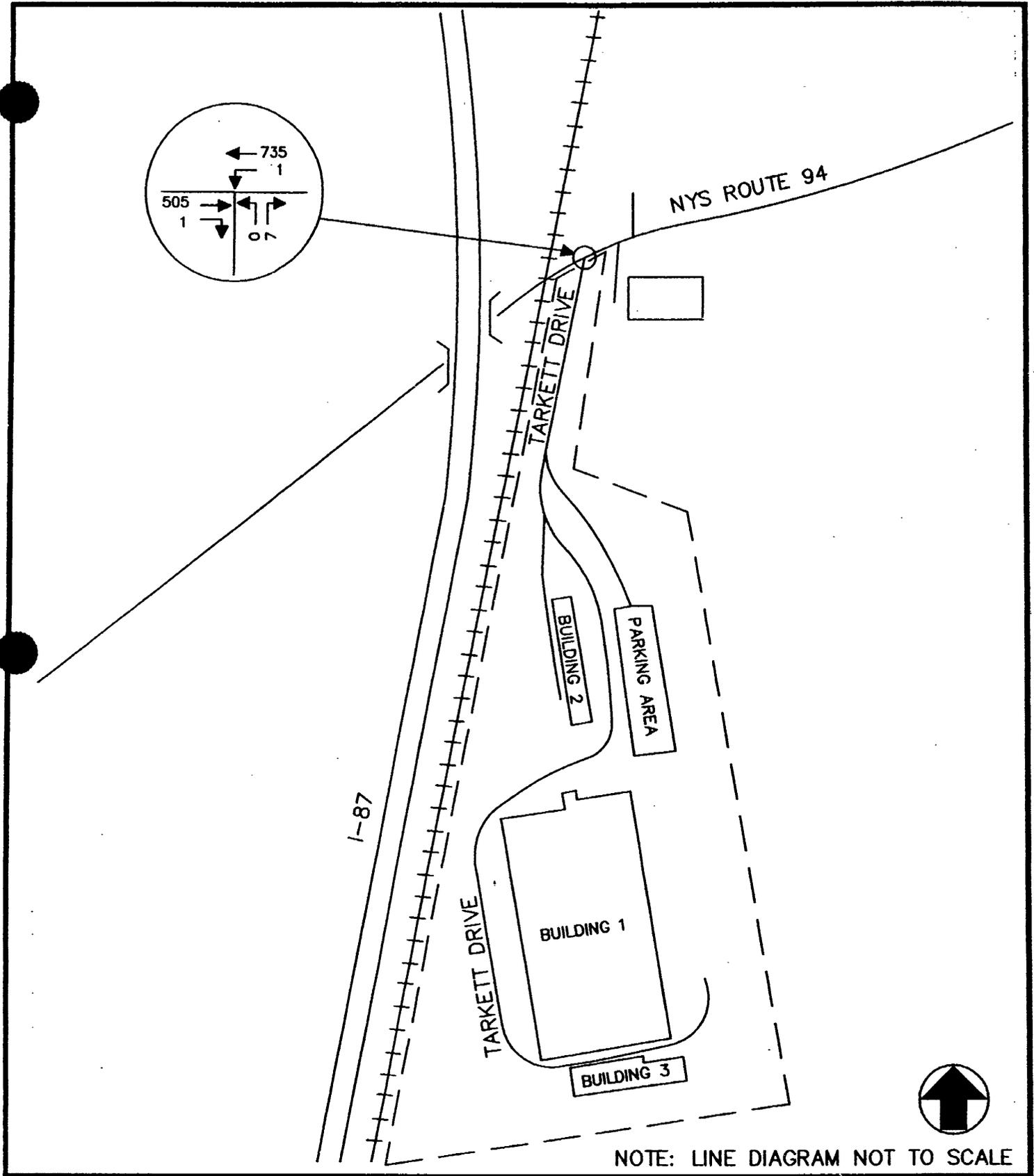
NOTE: LINE DIAGRAM NOT TO SCALE

**TARKETT PROPERTY REDEVELOPMENT**  
 TOWNS CORNWALL AND NEW WINDSOR, NEW YORK

**ARRIVAL DISTRIBUTION**

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PROJECT NO. 309 DATE: JULY, 2006 FIG. NO. 6



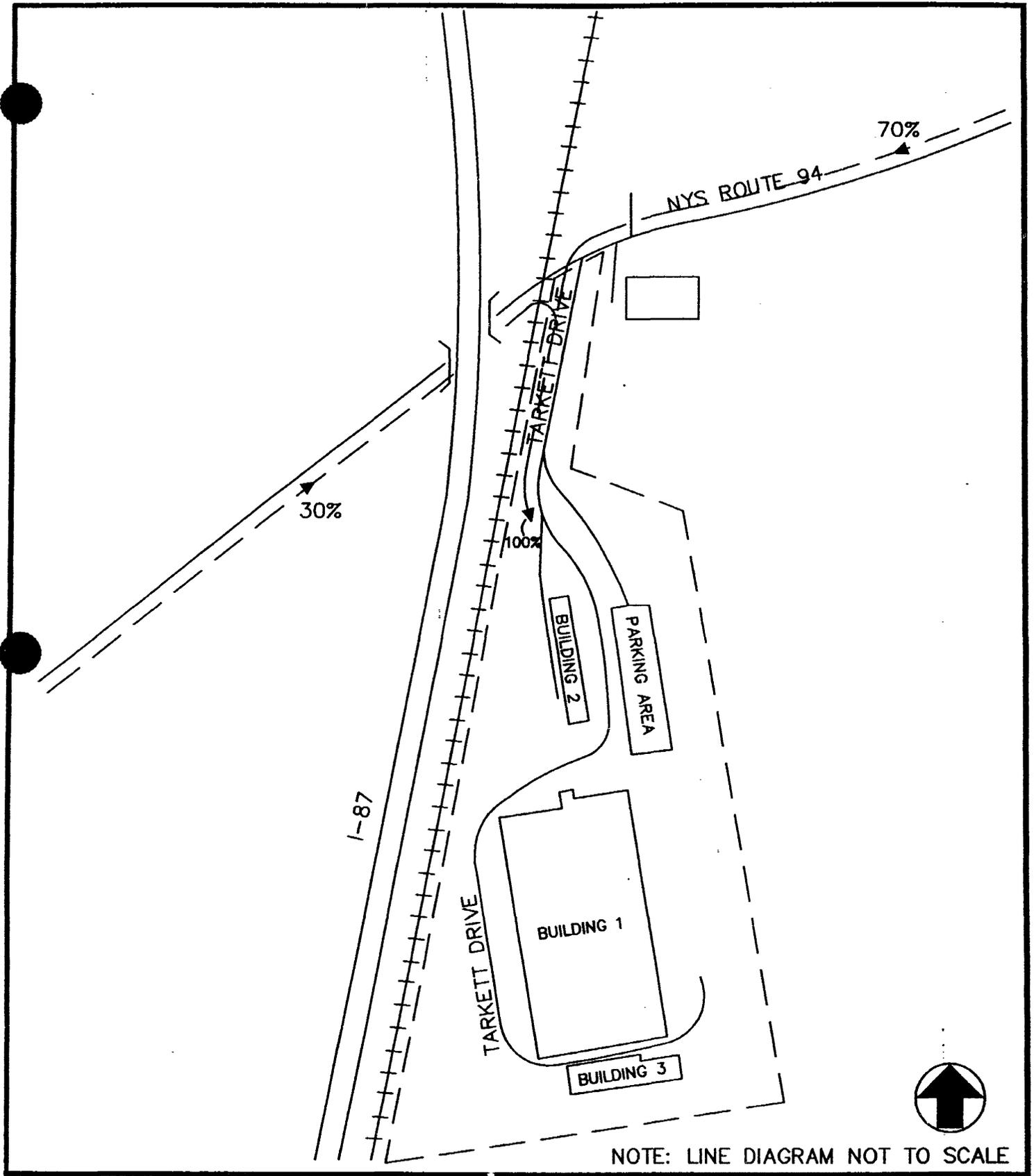
NOTE: LINE DIAGRAM NOT TO SCALE

TARKETT PROPERTY REDEVELOPMENT  
TOWNS CORNWALL AND NEW WINDSOR, NEW YORK

2009 NO-BUILD TRAFFIC VOLUMES  
WEEKDAY PEAK PM HOUR

JOHN COLLINS ENGINEERS, P.C.  
HAWTHORNE, NEW YORK

PROJECT NO. 309 DATE: JULY, 2006 FIG. NO. 5



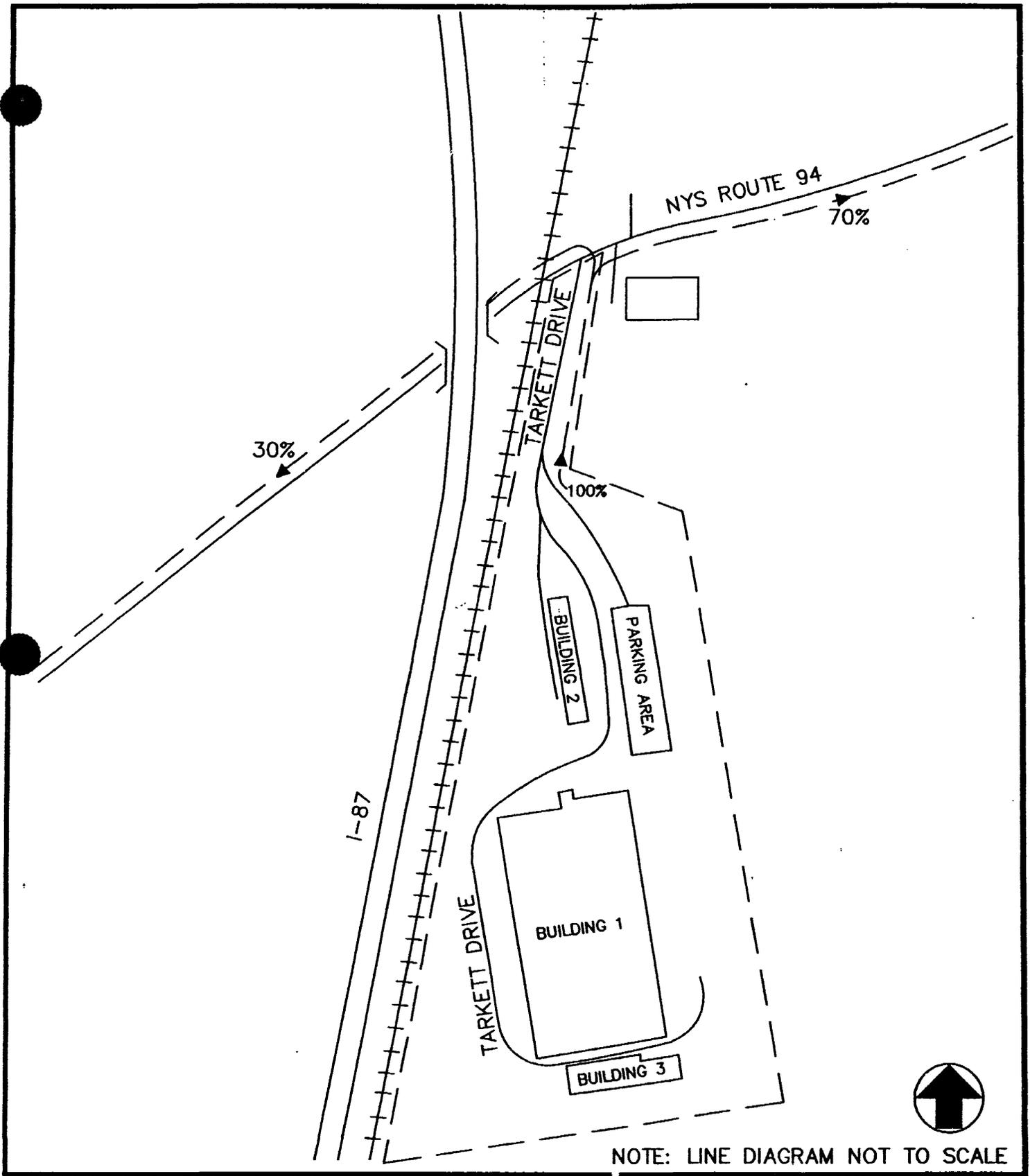
NOTE: LINE DIAGRAM NOT TO SCALE

**TARKETT PROPERTY REDEVELOPMENT**  
 TOWNS CORNWALL AND NEW WINDSOR, NEW YORK

**ARRIVAL DISTRIBUTION**

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 HAWTHORNE, NEW YORK

PROJECT NO. 309 DATE: JULY, 2006 FIG. NO. 6

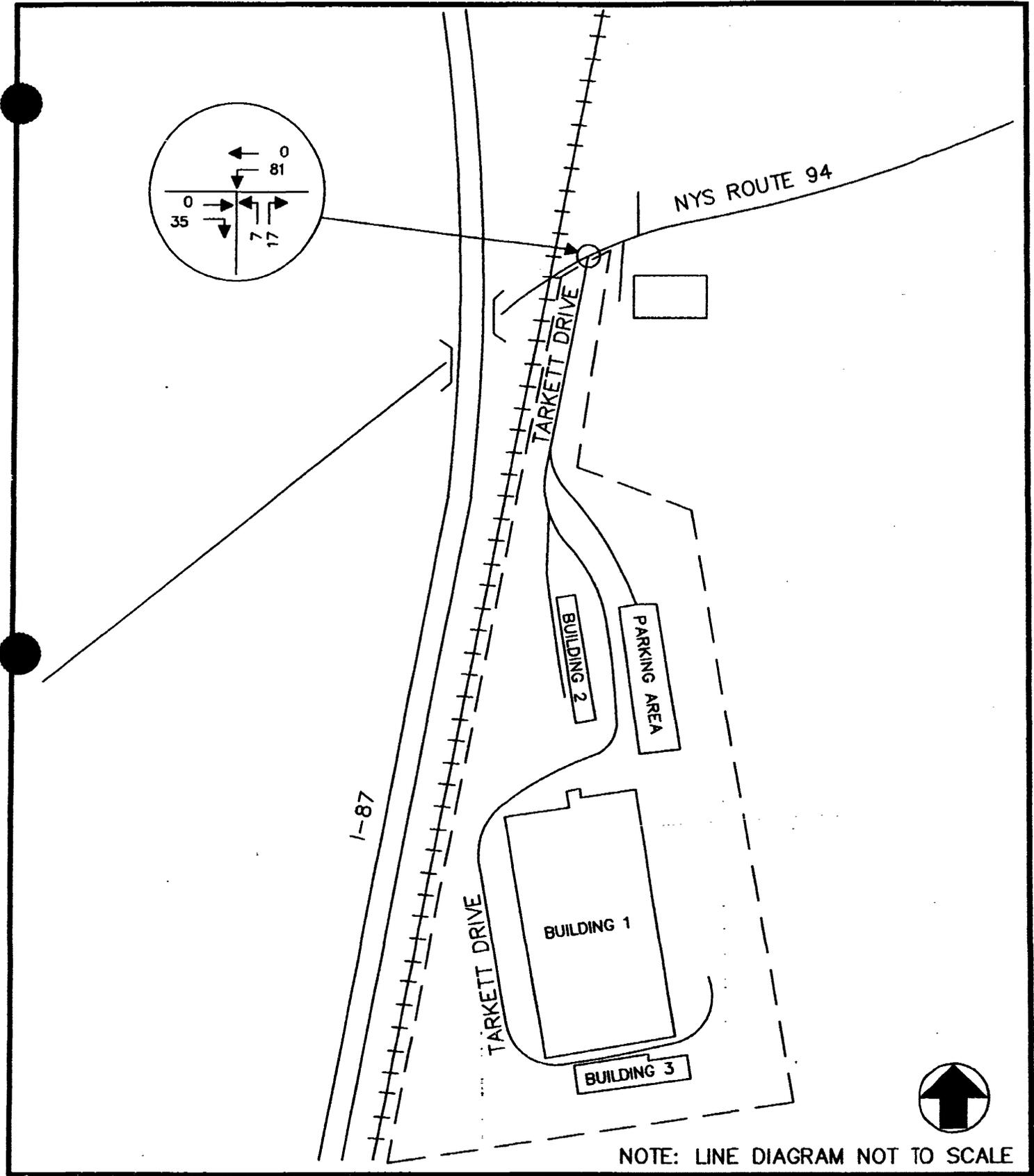


**TARKETT PROPERTY REDEVELOPMENT**  
 TOWNS CORNWALL AND NEW WINDSOR, NEW YORK

**DEPARTURE DISTRIBUTION**

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PROJECT NO. 309 DATE: JULY, 2006 FIG. NO. 7

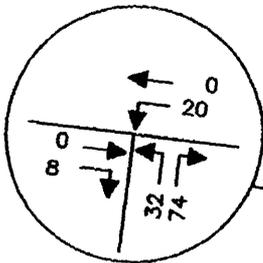
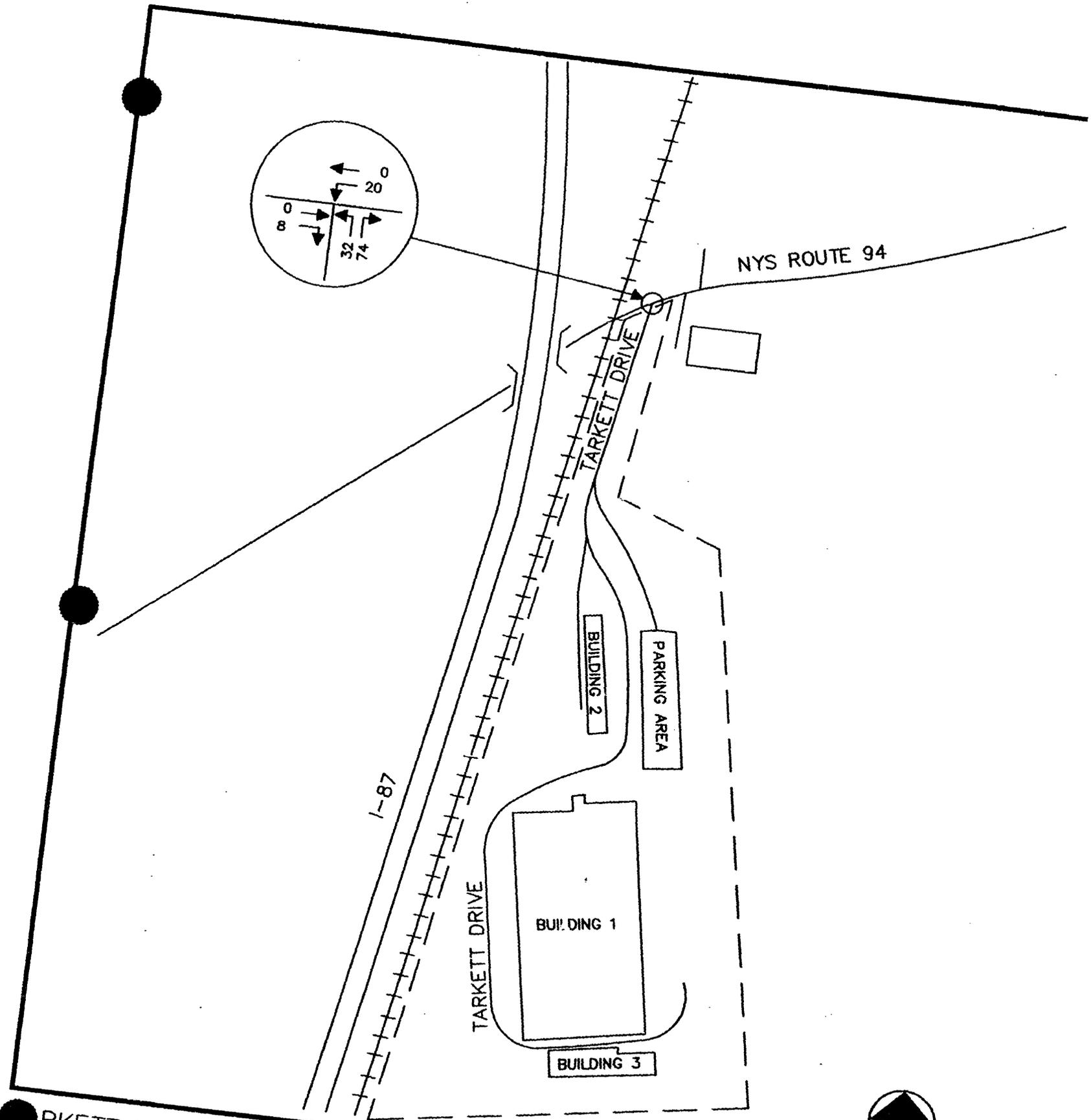


NOTE: LINE DIAGRAM NOT TO SCALE

**TARKETT PROPERTY REDEVELOPMENT SITE GENERATED TRAFFIC VOLUMES**  
**TOWNS CORNWALL AND NEW WINDSOR, NEW YORK WEEKDAY PEAK AM HOUR**

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 HAWTHORNE, NEW YORK

PROJECT NO. 309 DATE: JULY. 2006 FIG. NO. 8



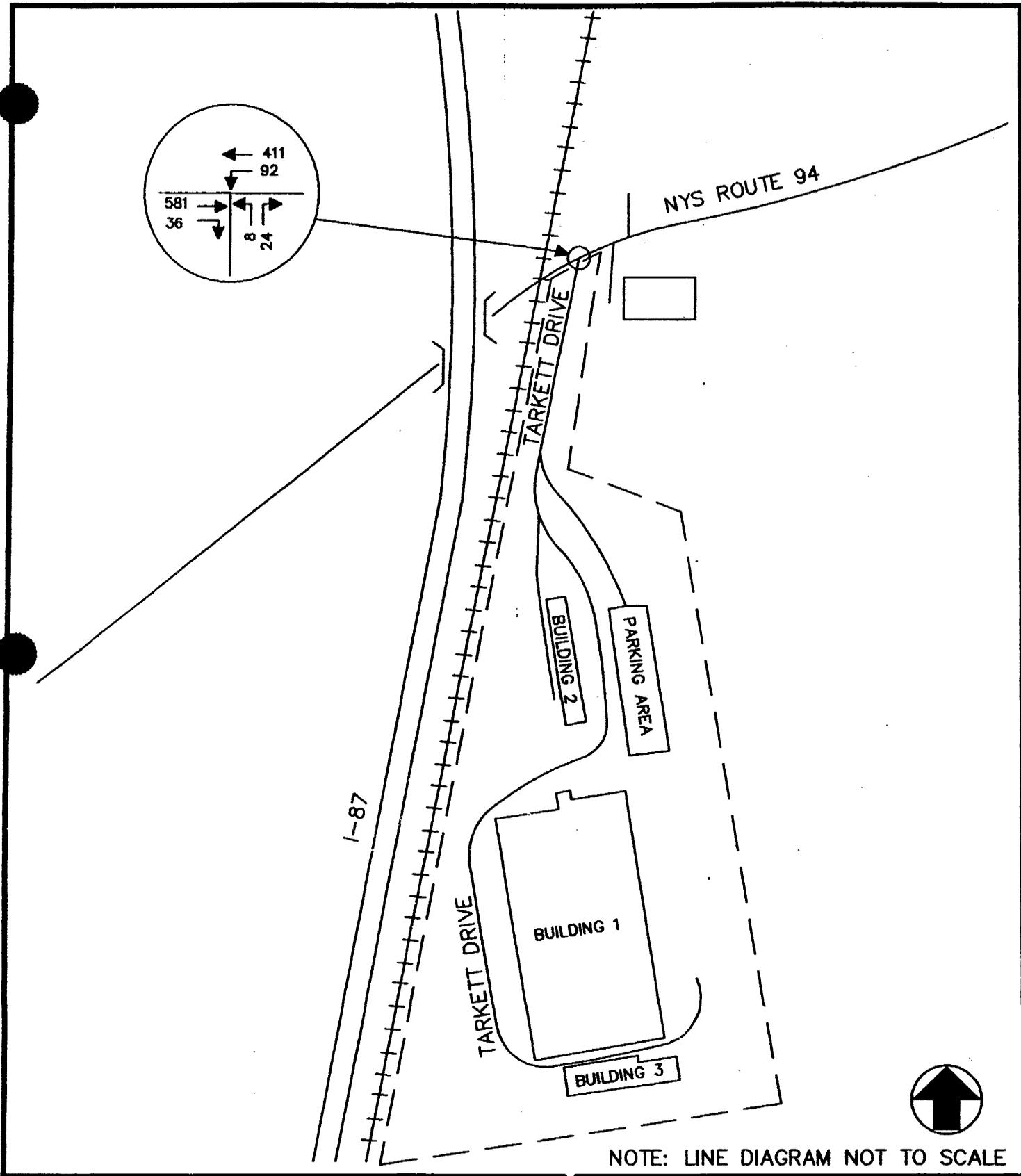
TARKETT PROPERTY REDEVELOPMENT  
 TOWNS CORNWALL AND NEW WINDSOR, NEW YORK  
 JOHN COLLINS ENGINEERS, P.C.  
 HAWTHORNE, NEW YORK

NOTE: LINE DIAGRAM NOT TO SCALE  
 SITE GENERATED TRAFFIC VOLUMES  
 WEEKDAY PEAK PM HOUR

PROJECT NO. 309 DATE: JULY. 2006 FIG. NO. 9

**APPENDIX "B"**

**TABLES**



NOTE: LINE DIAGRAM NOT TO SCALE

**TARKETT PROPERTY REDEVELOPMENT**  
 TOWNS CORNWALL AND NEW WINDSOR, NEW YORK

**2009 BUILD TRAFFIC VOLUMES**  
**WEEKDAY PEAK AM HOUR**

JOHN COLLINS ENGINEERS, P.C.  
 HAWTHORNE, NEW YORK

PROJECT NO. 309 DATE: JULY, 2006 FIG. NO. 10

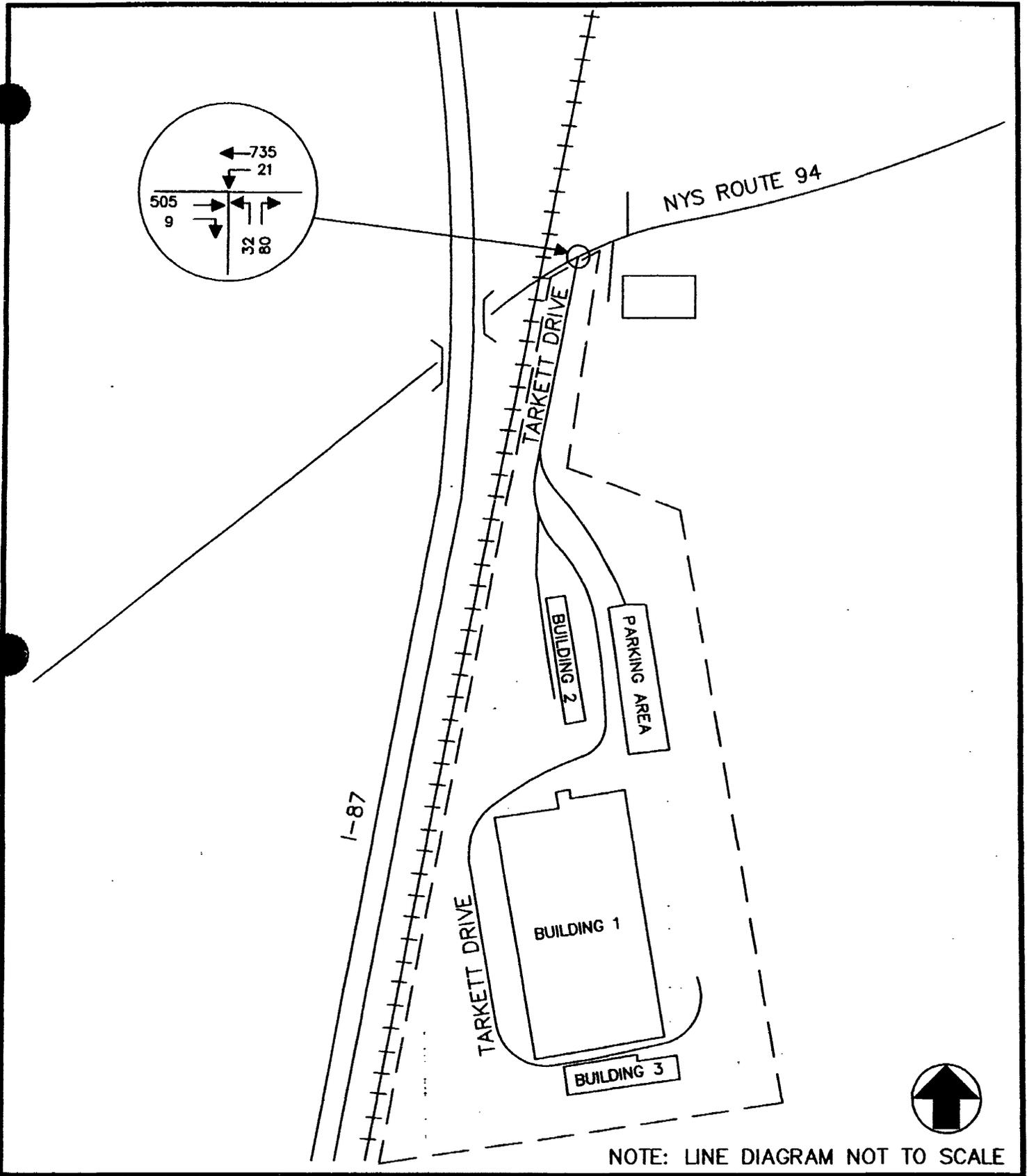
**TABLE 2**

**LEVEL OF SERVICE SUMMARY TABLE**

			2006 EXISTING		2009 NO-BUILD		2009 BUILD	
			AM	PM	AM	PM	AM	PM
1	NYS ROUTE 94 & TARKETT DRIVE	<b>UNSIGNALIZED</b> WB NB	A[8.8] B[13.2]	A[8.5] B[11.6]	A[8.9] B[13.9]	A[8.6] B[12.0]	A[9.5] C[18.1]	A[8.7] D[25.4]

NOTES:

THE ABOVE REPRESENTS THE LEVEL OF SERVICE AND VEHICLE DELAY IN SECONDS, C [16.2], FOR EACH APPROACH AS WELL AS FOR THE OVERALL INTERSECTION FOR THE SIGNALIZED INTERSECTIONS. SEE APPENDIX "D" FOR DESCRIPTIONS OF LEVELS OF SERVICE.



NOTE: LINE DIAGRAM NOT TO SCALE

TARKETT PROPERTY REDEVELOPMENT  
TOWNS CORNWALL AND NEW WINDSOR, NEW YORK

2009 BUILD TRAFFIC VOLUMES  
WEEKDAY PEAK PM HOUR

JOHN COLLINS ENGINEERS, P.C.  
HAWTHORNE, NEW YORK

PROJECT NO. 309 DATE: JULY. 2006 FIG. NO. 11

APPENDIX "B"

TABLES

TABLE 1

HOURLY TRIP GENERATION RATES (HTGR) AND ANTICIPATED  
SITE GENERATED TRAFFIC VOLUMES

TARKETT PROPERTY REDEVELOPMENT CORNWALL / NEW WINDSOR, NEW YORK	ENTRY		EXIT	
	HTGR*	VOLUME	HTGR*	VOLUME
(256,892 SQ. FT.)				
PEAK AM HIGHWAY HOUR	0.45	116	0.09	24
PEAK PM HIGHWAY HOUR	0.11	28	0.41	105

NOTES:

- 1) \* THE HOURLY TRIP GENERATION RATES (HTGR) ARE BASED ON THE DATA PUBLISHED BY THE INSTITUTE OF TRANSPORTATION ENGINEERS (ITE) AS CONTAINED IN THE TRIP GENERATION HANDBOOK, 7TH EDITION, 2003.

**TABLE 2**

**LEVEL OF SERVICE SUMMARY TABLE**

			2006 EXISTING		2009 NO-BUILD		2009 BUILD	
			AM	PM	AM	PM	AM	PM
1	NYS ROUTE 94 & TARKETT DRIVE	UNSIGNALIZED						
		WB	A[8.8]	A[8.5]	A[8.9]	A[8.6]	A[9.5]	A[8.7]
		NB	B[13.2]	B[11.6]	B[13.9]	B[12.0]	C[18.1]	D[25.4]

NOTES:

THE ABOVE REPRESENTS THE LEVEL OF SERVICE AND VEHICLE DELAY IN SECONDS, C [16.2], FOR EACH APPROACH AS WELL AS FOR THE OVERALL INTERSECTION FOR THE SIGNALIZED INTERSECTIONS. SEE APPENDIX "D" FOR DESCRIPTIONS OF LEVELS OF SERVICE.

APPENDIX "C"  
CAPACITY ANALYSIS

TWO-WAY STOP CONTROL SUMMARY

Analyst: RGD  
 Agency/Co.: JCE  
 Date Performed: 7/25/06  
 Analysis Time Period: AM PEAK HOUR  
 Intersection: NYS ROUTE 94 & TARKETT DRIVE  
 Jurisdiction:  
 Units: U. S. Customary  
 Analysis Year: 2009 NO-BUILD TRAFFIC VOLUMES  
 Project ID: 309AMNB1  
 East/West Street: NYS ROUTE 94  
 North/South Street: TARKETT DRIVE  
 Intersection Orientation: EW Study period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street: Approach Movement	Eastbound			Westbound		
	1 L	2 T	3 R	4 L	5 T	6 R

Volume	581	1	11	411		
Peak-Hour Factor, PHF	0.90	0.90	0.90	0.90		
Hourly Flow Rate, HFR	645	1	12	456		
Percent Heavy Vehicles	--	--	5	--	--	
Median Type/Storage	Undivided		/			
RT Channelized?						
Lanes	1	0		1	1	
Configuration		TR		L	T	
Upstream Signal?	No			No		

Minor Street: Approach Movement	Northbound			Southbound		
	7 L	8 T	9 R	10 L	11 T	12 R

Volume	1	8				
Peak Hour Factor, PHF	0.90	0.90				
Hourly Flow Rate, HFR	1	8				
Percent Heavy Vehicles	5	5				
Percent Grade (%)		0			0	
Flared Approach: Exists?/Storage		No	/		/	
Lanes	0	0				
Configuration		LR				

Delay, Queue Length, and Level of Service

Approach Movement	EB	WB	Northbound			Southbound		
	1	4	7	8	9	10	11	12

Lane Config	L		LR					
v (vph)	12		9					
C(m) (vph)	925		415					
v/c	0.01		0.02					
95% queue length	0.04		0.07					
Control Delay	8.9		13.9					
Approach Delay	A		B					
Approach LOS			B					

TWO-WAY STOP CONTROL SUMMARY

Analyst: RGD  
 Agency/Co.: JCE  
 Date Performed: 6/20/2006  
 Analysis Time Period: PM PEAK HOUR  
 Intersection: NYS ROUTE 94 & TARKETT DRIVE  
 Jurisdiction:  
 Units: U. S. Customary  
 Analysis Year: 2006 EXISTING TRAFFIC VOLUMES  
 Project ID: 309PMEX1  
 East/West Street: NYS ROUTE 94  
 North/South Street: TARKETT DRIVE  
 Intersection Orientation: EW Study period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street:	Approach Movement	Eastbound				Westbound		
		1 L	2 T	3 R	4 L	5 T	6 R	
Volume		463	1	1	674			
Peak-Hour Factor, PHF		0.90	0.90	0.90	0.90			
Hourly Flow Rate, HFR		514	1	1	748			
Percent Heavy Vehicles		--	--	5	--	--		
Median Type/Storage		Undivided		/				
RT Channelized?								
Lanes		1	0		1	1		
Configuration			TR		L	T		
Upstream Signal?		No			No			

Minor Street:	Approach Movement	Northbound			Southbound		
		7 L	8 T	9 R	10 L	11 T	12 R
Volume		0	6				
Peak Hour Factor, PHF		0.90	0.90				
Hourly Flow Rate, HFR		0	6				
Percent Heavy Vehicles		5	5				
Percent Grade (%)		0			0		
Flared Approach: Exists?/Storage			No	/		/	
Lanes		0	0				
Configuration			LR				

Delay, Queue Length, and Level of Service

Approach	EB	WB	Northbound			Southbound		
			7	8	9	10	11	12
Movement	1	4	7	8	9	10	11	12
Lane Config		L		LR				
v (vph)		1		6				
C(m) (vph)		1035		555				
v/c		0.00		0.01				
95% queue length		0.00		0.03				
Control Delay		8.5		11.6				
LOS		A		B				
Approach Delay				11.6				
Approach LOS				B				

TWO-WAY STOP CONTROL SUMMARY

Analyst: RGD  
 Agency/Co.: JCE  
 Date Performed: 7/25/06  
 Analysis Time Period: AM PEAK HOUR  
 Intersection: NYS ROUTE 94 & TARKETT DRIVE  
 Jurisdiction:  
 Units: U. S. Customary  
 Analysis Year: 2009 NO-BUILD TRAFFIC VOLUMES  
 Project ID: 309AMNB1  
 East/West Street: NYS ROUTE 94  
 North/South Street: TARKETT DRIVE  
 Intersection Orientation: EW Study period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street:	Approach Movement	Eastbound			Westbound		
		1 L	2 T	3 R	4   L	5 T	6 R
Volume		581	1		11	411	
Peak-Hour Factor, PHF		0.90	0.90		0.90	0.90	
Hourly Flow Rate, HFR		645	1		12	456	
Percent Heavy Vehicles		--	--		5	--	--
Median Type/Storage		Undivided			/		
RT Channelized?							
Lanes		1	0		1	1	
Configuration			TR		L	T	
Upstream Signal?		No				No	

Minor Street:	Approach Movement	Northbound			Southbound		
		7 L	8 T	9 R	10   L	11 T	12 R
Volume		1		8			
Peak Hour Factor, PHF		0.90		0.90			
Hourly Flow Rate, HFR		1		8			
Percent Heavy Vehicles		5		5			
Percent Grade (%)			0			0	
Flared Approach: Exists?/Storage				No	/		/
Lanes		0		0			
Configuration			LR				

Delay, Queue Length, and Level of Service

Approach Movement	EB	WB	Northbound			Southbound		
			4 	7	8	9	10 	11
Lane Config	1	L		LR				
v (vph)		12		9				
C(m) (vph)		925		415				
v/c		0.01		0.02				
95% queue length		0.04		0.07				
Control Delay		8.9		13.9				
Approach Delay		A		B				
Approach LOS				13.9				
				B				

TWO-WAY STOP CONTROL SUMMARY

Analyst: RGD  
 Agency/Co.: JCE  
 Date Performed: 7/25/06  
 Analysis Time Period: PM PEAK HOUR  
 Intersection: NYS ROUTE 94 & TARKETT DRIVE  
 Jurisdiction:  
 Units: U. S. Customary  
 Analysis Year: 2009 NO-BUILD TRAFFIC VOLUMES  
 Project ID: 309PMNB1  
 East/West Street: NYS ROUTE 94  
 North/South Street: TARKETT DRIVE  
 Intersection Orientation: EW Study period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street:	Approach Movement	Eastbound			Westbound		
		1 L	2 T	3 R	4 L	5 T	6 R
Volume			505	1	1	735	
Peak-Hour Factor, PHF			0.90	0.90	0.90	0.90	
Hourly Flow Rate, HFR			561	1	1	816	
Percent Heavy Vehicles			--	--	5	--	--
Median Type/Storage		Undivided			/		
RT Channelized?							
Lanes			1	0		1	1
Configuration				TR		L	T
Upstream Signal?			No			No	

Minor Street:	Approach Movement	Northbound			Southbound		
		7 L	8 T	9 R	10 L	11 T	12 R
Volume		0		7			
Peak Hour Factor, PHF		0.90		0.90			
Hourly Flow Rate, HFR		0		7			
Percent Heavy Vehicles		5		5			
Percent Grade (%)			0			0	
Flared Approach: Exists?/Storage				No	/		/
Lanes		0		0			
Configuration			LR				

Delay, Queue Length, and Level of Service

Approach Movement	EB	WB	Northbound			Southbound		
			4	7	8	9	10	11
Lane Config	1	L		LR				
v (vph)		1		7				
C(m) (vph)		995		521				
v/c		0.00		0.01				
95% queue length		0.00		0.04				
Control Delay		8.6		12.0				
LOS		A		B				
Approach Delay				12.0				
Approach LOS				B				

TWO-WAY STOP CONTROL SUMMARY

Analyst: RGD  
 Agency/Co.: JCE  
 Date Performed: 7/25/06  
 Analysis Time Period: AM PEAK HOUR  
 Intersection: NYS ROUTE 94 & TARKETT DRIVE  
 Jurisdiction:  
 Units: U. S. Customary  
 Analysis Year: 2009 BUILD TRAFFIC VOLUMES  
 Project ID: 309AMBD1  
 East/West Street: NYS ROUTE 94  
 North/South Street: TARKETT DRIVE  
 Intersection Orientation: EW Study period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street: Approach Movement	Eastbound				Westbound		
	1 L	2 T	3 R	4 L	5 T	6 R	
Volume		581	36	92	411		
Peak-Hour Factor, PHF		0.90	0.90	0.90	0.90		
Hourly Flow Rate, HFR		645	40	102	456		
Percent Heavy Vehicles		--	--	5	--	--	
Median Type/Storage RT Channelized?	Undivided			/			
Lanes		1	0		1	1	
Configuration			TR		L	T	
Upstream Signal?		No			No		

Minor Street: Approach Movement	Northbound			Southbound		
	7 L	8 T	9 R	10 L	11 T	12 R
Volume	8		24			
Peak Hour Factor, PHF	0.90		0.90			
Hourly Flow Rate, HFR	8		26			
Percent Heavy Vehicles	5		5			
Percent Grade (%)		0			0	
Flared Approach: Exists?/Storage			No	/		/
Lanes	0		0			
Configuration		LR				

Delay, Queue Length, and Level of Service

Approach Movement Lane Config	EB	WB	Northbound			Southbound		
	1	4	7	8	9	10	11	12
v (vph)		102		34				
C(m) (vph)		895		308				
v/c		0.11		0.11				
95% queue length		0.38		0.37				
Control Delay		9.5		18.1				
LOS		A		C				
Approach Delay				18.1				
Approach LOS				C				

TWO-WAY STOP CONTROL SUMMARY

Analyst: RGD  
 Agency/Co.: JCE  
 Date Performed: 7/25/06  
 Analysis Time Period: PM PEAK HOUR  
 Intersection: NYS ROUTE 94 & TARKETT DRIVE  
 Jurisdiction:  
 Units: U. S. Customary  
 Analysis Year: 2009 BUILD TRAFFIC VOLUMES  
 Project ID: 309PMBD1  
 East/West Street: NYS ROUTE 94  
 North/South Street: TARKETT DRIVE  
 Intersection Orientation: EW Study period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street:	Approach Movement	Eastbound				Westbound		
		1 L	2 T	3 R	4   L	5 T	6 R	
Volume		505	9	21	735			
Peak-Hour Factor, PHF		0.90	0.90	0.90	0.90			
Hourly Flow Rate, HFR		561	10	23	816			
Percent Heavy Vehicles		--	--	5	--	--		
Median Type/Storage		Undivided			/			
RT Channelized?								
Lanes		1	0		1	1		
Configuration			TR		L	T		
Upstream Signal?		No			No			

Minor Street:	Approach Movement	Northbound				Southbound		
		7 L	8 T	9 R	10   L	11 T	12 R	
Volume		32	80					
Peak Hour Factor, PHF		0.90	0.90					
Hourly Flow Rate, HFR		35	88					
Percent Heavy Vehicles		5	5					
Percent Grade (%)		0		0				
Flared Approach: Exists?/Storage		No		/		/		
Lanes		0	0					
Configuration		LR						

Delay, Queue Length, and Level of Service

Approach	EB	WB	Northbound			Southbound		
			4	7	8	9	10	11
Movement	1	4	7	8	9	10	11	12
Lane Config		L		LR				
v (vph)		23		123				
C(m) (vph)		987		297				
v/c		0.02		0.41				
95% queue length		0.07		1.95				
Control Delay		8.7		25.4				
Approach Delay		A		D				
Approach LOS				25.4				
				D				

APPENDIX "D"

STANDARDS

LEVEL OF SERVICE FOR SIGNALIZED INTERSECTIONS

Level of Service (LOS) for signalized intersections is defined in terms of control delay, which is a measure of driver discomfort, frustration, fuel consumption, and increased travel time. The delay experienced by a motorist is made up of a number of factors that relate to control, geometrics, traffic, and incidents. Specifically, LOS criteria for traffic signals are stated in terms of the average control delay per vehicle, typically for a 15-minute analysis period. The criteria are given in Exhibit 16-2 from the 2000 Highway Capacity Manual published by the Transportation Research Board.

EXHIBIT 16-2

LEVEL OF SERVICE FOR SIGNALIZED INTERSECTIONS

LEVEL OF SERVICE (LOS)	CONTROL DELAY PER VEHICLE (S/VEH)
A	≤10
B	>10-20
C	>20-35
D	>35-55
E	>55-80
F	>80

LEVEL OF SERVICE A describes operations with low control delay, up to 10 seconds per vehicle (s/veh). This LOS occurs when progression is extremely favorable and most vehicles arrive during the green phase. Many vehicles do not stop at all. Short cycle lengths may tend to contribute to low delay values.

LEVEL OF SERVICE B describes operations with control delay greater than 10 and up to 20 seconds per vehicle (s/veh). This level generally occurs with good progression, short cycle lengths, or both. More vehicles stop than with Level of Service "A", causing higher levels of delay.

LEVEL OF SERVICE C describes operations with control delay greater than 20 and up to 35 seconds per vehicle (s/veh). These higher delays may result from only fair progression, longer cycle lengths, or both. Individual cycle failures may begin to appear at this level. The number of vehicles stopping is significant at this level, though many still pass through the intersection without stopping.

LEVEL OF SERVICE D describes operations with control delay greater than 35 and up to 55 seconds per vehicle (s/veh). At Level of Service D, the influence of congestion becomes more noticeable. Longer delays may result from some combination of unfavorable progression, long cycle lengths, and high v/c ratios. Many vehicles stop, and the proportion of vehicles not stopping declines. Individual cycle failures are noticeable.

LEVEL OF SERVICE E describes operations with control delay greater than 55 and up to 80 seconds per vehicle (s/veh). This is considered to be the limit of acceptable delay. These high delay values generally indicate poor progression, long cycle lengths, and high v/c ratios. Individual cycle failures are frequent.

LEVEL OF SERVICE F describes operations with control delay in excess of 80 seconds per vehicle (s/veh). This level is considered unacceptable to most drivers, often occurs with oversaturation, that is, when arrival flow rates exceed the capacity of the groups. It may also occur at high v/c ratios with many individual cycle failures. Poor progression and long cycle lengths may also contribute significantly to high delay levels.

LEVEL OF SERVICE CRITERIA FOR UNSIGNALIZED INTERSECTIONS

The Level of Service (LOS) for unsignalized intersections is determined by the computed or measured control delay and is defined for each minor movement. Control delay is defined as the total elapsed time a vehicle stops at the end of the queue to the time the vehicle departs from the stop line. This total elapsed time includes the time required for the vehicle to travel from the last-in-queue position to the first-in-queue position, including deceleration of vehicles from free-flow speed to speed of vehicles in queue. Average control delay for any particular minor movement is a function of the capacity of the approach and the degree of saturation. The Level of Service Criteria are given in Exhibit 17-2 from the 2000 Highway Capacity Manual published by the Transportation Research Board.

EXHIBIT 17-2

LEVEL OF SERVICE FOR CRITERIA  
FOR UNSIGNALIZED INTERSECTIONS

LEVEL OF SERVICE (LOS)	AVERAGE CONTROL DELAY (S/VEH)
A	0-10
B	>10-15
C	>15-25
D	>25-35
E	>35-50
F	>50

The Level of Service Criteria for unsignalized intersections are somewhat different from the criteria for signalized intersections.

# JOHN COLLINS ENGINEERS, P.C.

TRAFFIC • TRANSPORTATION ENGINEERS

===== 11 BRADHURST AVENUE • HAWTHORNE, N.Y. • 10532 • (914) 347-7500 • FAX (914) 347-7266 =====

## MEMORANDUM

TO: Mark Edsall, P.E.  
FROM: Philip J. Grealy, Ph.D., P.E.   
DATE: December 19, 2006  
SUBJECT: Response to Comments  
PROJECT: No. 309

\*\*\*\*\*

### Comments on Traffic Study Vail Gates Business Park (Tarkett Redevelopment)

The following items are in response to your comments regarding our traffic report and your request for additional information.

1. Page 3 has been corrected to indicate the terminus of Route 94 in the Town of New Windsor.
2. Page 5 – The Table No. 1 has been updated to include the ITE trip classifications.
3. Page 5 – While data are not available to us on the exact trip generation of the former Tarkett operation, estimates of the traffic volumes were computed for the former use based on the comparable ITE Land Use and these are shown in Table 1-T. Note that due to the former shift operation, it is likely that the peak volumes were even more concentrated.
4. Page 6 – Other development traffic was accounted for as part of the growth factor utilized in the analysis. This would account for such projects as the

Chestnut Woods, Cornwall Commons and other miscellaneous developments traffic in the area.

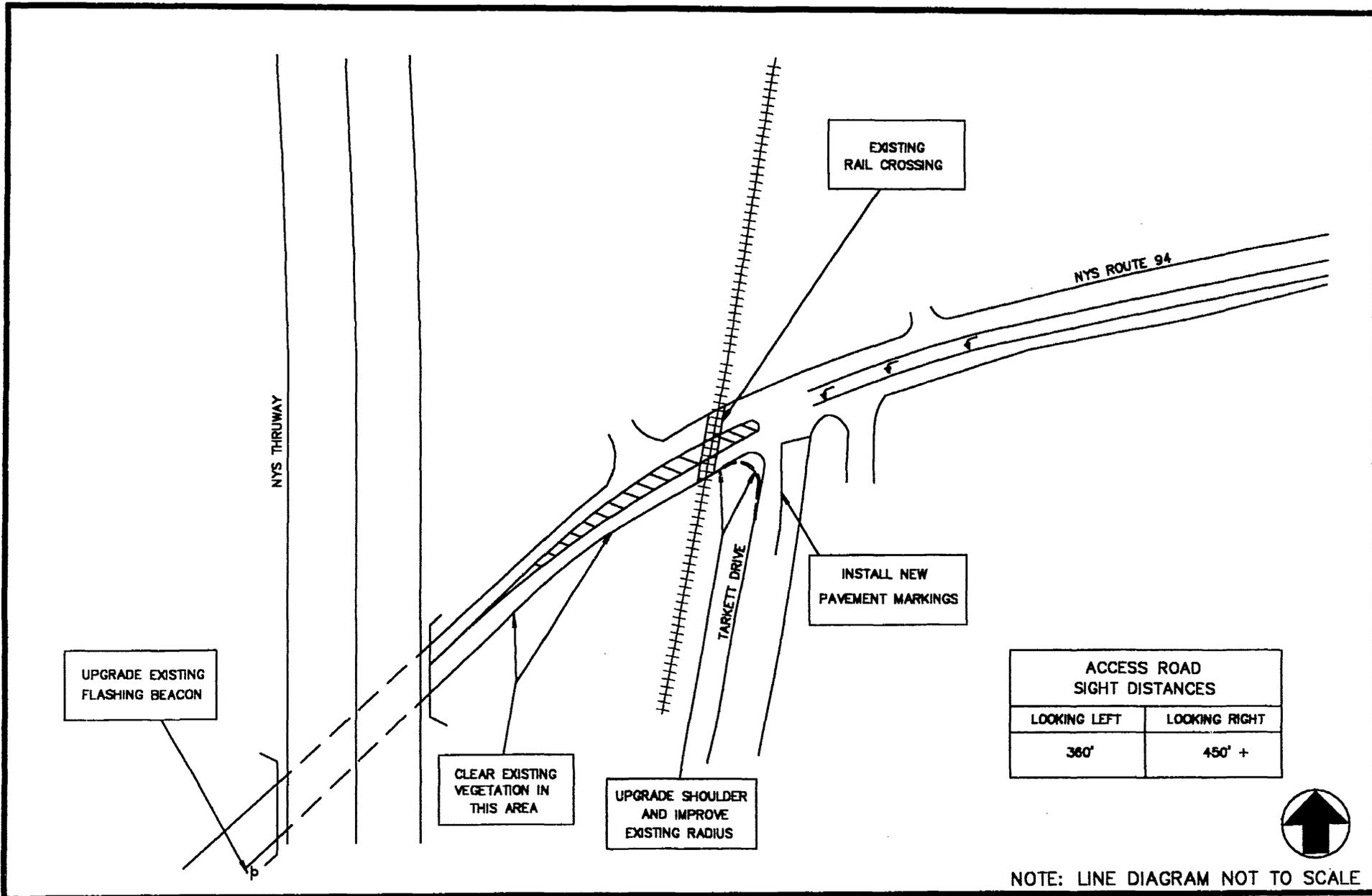
5. At this time, the removal of the railroad crossing on Route 94 will not occur since it is controlled by CSX. However, since this crossing is not active, we are pursuing the possibility of getting an "Exempt" posting which would not require school buses to stop (see information attached from the NYSMUTCD Section 220.3 regarding this).
6. A conceptual plan has been prepared for access improvements in addition to Item 5 (see Figure No. 1A). These will also be coordinated with the New York State Department of Transportation as part of a Highway Work Permit.
7. The sight distances at the entrance drive are shown on Figure 1A. The sight distance improvements will ensure that the sight lines are kept clear during all times of the year. This distance is consistent with the NYSDOT stopping sight distances.
8. The traffic volumes shown in the trip generation tables reflect passenger cars and trucks based on available ITE data.

**TABLE 1**  
**HOURLY TRIP GENERATION RATES (HTGR) AND ANTICIPATED**  
**SITE GENERATED TRAFFIC VOLUMES**

TARKETT PROPERTY NEW WINDSOR, NEW YORK	ENTRY				EXIT			
	HTGR*	VOLUME	PASSENGER CARS	TRUCKS	HTGR*	VOLUME	PASSENGER CARS	TRUCKS
LIGHT MANUFACTURING/WAREHOUSE (256,892 S.F.)								
PEAK AM HOUR	0.45	116	110	6	0.09	24	23	1
PEAK PM HOUR	0.11	28	27	1	0.41	105	100	5

NOTES:

- 1) \* THE HOURLY TRIP GENERATION RATES (HTGR) ARE BASED ON THE DATA PUBLISHED BY THE INSTITUTE TRANSPORTATION ENGINEERS (ITE) AS CONTAINED IN THE TRIP GENERATION HANDBOOK, 7TH EDITION, JANUARY 2003. LAND USES 120 AND 150.



**TARKETT PROPERTY  
NEW WINDSOR, NEW YORK**

**CONCEPTUAL IMPROVEMENT PLAN**

JOHN COLLINS ENGINEERS, P.C.  
HAWTHORNE, NEW YORK

PROJECT NO. 309 DATE: DECEMBER 2006 FIG. NO.1A

**TABLE 1 T**  
**HOURLY TRIP GENERATION RATES (HTGR) AND ANTICIPATED**  
**SITE GENERATED TRAFFIC VOLUMES**  
**FORMER TARKETT USE**

FORMER TARKETT USE NEW WINDSOR, NEW YORK	ENTRY				EXIT			
	HTGR*	VOLUME	PASSENGER CARS	TRUCKS	HTGR*	VOLUME	PASSENGER CARS	TRUCKS
MANUFACTURING (256,892 S.F.)								
PEAK AM HOUR	0.55	142	131	11	0.16	42	39	3
PEAK PM HOUR	0.26	67	62	5	0.47	120	110	10

**NOTES:**

- 1) \* VOLUMES REPRESENT ESTIMATES OF FORMER USE BASED ON THE HOURLY TRIP GENERATION RATES (HTGR) DATA PUBLISHED BY THE INSTITUTE TRANSPORTATION ENGINEERS (ITE) AS CONTAINED IN THE TRIP GENERATION HANDBOOK, 7TH EDITION, JANUARY 2003. FOR MANUFACTURING LAND USE 140.

**PART 220****RAILROAD CROSSING SIGNS**

(Statutory authority: Vehicle and Traffic Law, § 1680[a]; Transportation Law, § 14[18])

Sec.	
220.1	Railroad crossbuck sign
220.2	Tracks sign
220.3	Exempt sign
220.4	High speed trains sign
220.5	Increased train traffic sign
220.6	Tracks out of service sign

**Historical Note**

Part (§§ 220.1-220.4) filed June 29, 1983 eff. July 1, 1983.

**§ 220.1 Railroad crossbuck sign.****White background  
Black legend****R10-1**

Sign No.	Panel	Size	Letter Size and Series
R10-1C	Rear	48" x 9"	5½"-D
	Front	48" x 9"	5½"-D

(a) *Application.* This sign is for use as the "sign board" required by section 53 of the Railroad Law to be placed at railroad-highway grade crossings (see section 200.4[e] of this Title). It shall be used at all crossings, except where the Department of Transportation authorizes its omission or a different installation.

(b) *Location.* (1) The R10-1 sign shall be located on the right side of the roadway in advance of the crossing, facing approaching traffic in each direction. A supplementary sign may be placed on the left side of the roadway in advance of and/or beyond the crossing.

(2) The R10-1 sign should be located as close to the crossing as practicable. However, it should be at least 12 feet from the nearest track center line, measured perpendicular to the track.

(c) *Height.* The lowest point of the R10-1 sign should be at least seven feet three inches above the near edge of roadway.

(d) *Illustrations.* Figures 240-2, 263-33, and 279-8 through 279-11 of this Title show examples of railroad crossbuck sign use.

(e) *Supplemental and related devices.* Tracks sign (see section 220.2 of this Part); exempt sign (see section 220.3 of this Part); high speed trains sign (see section 220.4 of this Part); increased train traffic sign (see section 220.5 of this Part); tracks out of service sign (see section 220.6 of this Part); railroad advance warning signs (see section 235.8 of this Title); railroad-

§ 220.1

TITLE 17 TRANSPORTATION

highway grade crossing markings (see section 262.19 of this Title); and railroad flashing signals, gates, and traffic control signals (see sections 277.6 through 277.8 of this Title).

**Historical Note**

Sec. filed June 29, 1983; ams. filed: March 26, 2001; Feb. 18, 2005 eff. March 9, 2005. Amended (e).

§ 220.2 Tracks sign.

**White background  
Black legend**



Sign No.	Panel	Size	Letter Size and Series
R10-2C	Top	9" x 9"	5½"-D
	Bottom	27" x 9"	4"-D

(a) *Application.* The R10-2 sign shall be used to supplement the railroad crossbuck sign (see section 220.1 of this Part) where a railroad-highway grade crossing consists of two or more tracks, including sidings.

(b) *Location.* The R10-2 sign shall be placed immediately below the R10-1 sign.

(c) *Height.* The bottom of the R10-2 sign should be at least six feet above the near edge of roadway.

(d) *Illustrations.* Figures 279-8 through 279-11 show examples of tracks sign use.

(e) *Supplemental and related devices.* Railroad crossbuck sign (see section 220.1 of this Part).

**Historical Note**

Sec. filed June 29, 1983; amd. filed March 26, 2001 eff. April 11, 2001. Amended (c).

§ 220.3 Exempt sign.

**White background  
Black legend**



Sign No.	Size	Margin	Border	Letter Size and Series
R10-3C	24" x 12"	¾"	¾"	4"-D

(a) *Application.* This sign is for use at railroad-highway grade crossings where the statutory stop normally required for certain vehicles by section 1171 of the Vehicle and Traffic Law has been legally waived. It is intended for use at abandoned crossings, infrequently used crossings,

and others where the statutory stop requirement is judged unnecessary. It may be used only where authorized by the Department of Transportation.

(b) *Location.* Where used, the R10-3 sign shall be placed in combination with the railroad crossbuck sign located on the right side of the roadway. It may also be used with supplementary crossbuck signs on the left side of the roadway.

(c) *Height.* The R10-3 sign shall be installed beneath the R10-1 sign. Where an R10-2 sign is used, the R10-3 shall be installed beneath it. Where a railroad flashing signal is used, the R10-3 sign shall be installed below the signal background panels. The bottom of the R10-3 sign should be at least six feet above the near edge of roadway.

(d) *Supplemental and related devices.* W5-15 exempt sign (see section 235.9 of this Title).

**Historical Note**

Sec. filed June 29, 1983; amd. filed March 26, 2001 eff. April 11, 2001. Amended (b)-(c), added (d).

**§ 220.4 High speed trains sign.**

**White background  
Black legend**



R10-4

Sign No.	Size	Margin	Border	Letter Size and Series		
				Line 1	Line 2	Line 3
R10-4C	24"×24"	3/8"	3/8"	4"-D	4"-D	4"-D

(a) *Application.* This sign is for use at railroad-highway grade crossings where it is deemed necessary to inform motorists that trains may approach at unusually high speed. It may be used only where authorized by the Department of Transportation. It shall not be used at crossings where R10-3 signs (see section 220.3 of this Part) are used.

(b) *Location.* Where used, the R10-4 sign shall be placed in combination with the railroad crossbuck sign located on the right side of the approach roadway. It may also be used with supplementary crossbuck signs on the left side of the roadway.

(c) *Height.* (1) Where an R10-1 sign is the only other device used, the R10-4 sign shall be placed immediately beneath it. The bottom of the R10-4 sign should be at least six feet above the near edge of roadway.

(2) Where an R10-2 sign is also used, the R10-4 sign shall be placed beneath it. The bottom of the R10-4 sign should be at least five feet six inches above the near edge of roadway.

(3) Where a railroad flashing signal is used, the R10-4 sign shall be placed below the signal background panels. The bottom of the R10-4 sign should be at least five feet six inches above the near edge of roadway.

**Historical Note**

Sec. filed June 29, 1983; amd. filed March 26, 2001 eff. April 11, 2001. Amended (b), added (c).

**1170. Obedience to signal indicating approach of train.** (a) Whenever any one driving a vehicle approaches a railroad grade crossing under any of the circumstances stated in this section, the driver of such vehicle shall stop not less than ten feet from the nearest rail of such railroad, and shall not proceed until he can do so safely. The foregoing requirements shall apply when:

1. An audible or clearly visible electric or mechanical signal device gives warning of the immediate approach of a railroad train;

2. A crossing gate is lowered or when a human flagman gives or continues to give signal of the approach or passage of a railroad train;

3. A railroad train approaching within approximately one thousand five hundred feet of a highway crossing emits a signal audible from such distance and such railroad train, by reason of its speed or nearness to such crossing, is an immediate hazard; or

4. An approaching railroad train is plainly visible and is in hazardous proximity to such crossing.

No person shall drive any vehicle through, around, or under any crossing gate or barrier at a railroad crossing while such gate or barrier is closed or is being opened or closed. Every person convicted of a violation of this subdivision shall for a first conviction be punished by a fine of not less than two hundred fifty dollars nor more than four hundred fifty dollars or by imprisonment for not more than thirty days or by both such fine and imprisonment; for a conviction of a second violation, both of which were committed within a period of eighteen months, such person shall be punished by a fine of not less than three hundred fifty dollars nor more than five hundred dollars or by imprisonment for not more than ninety days or by both such fine and imprisonment; upon conviction of a third or subsequent violation, all of which were committed within a period of eighteen months, such person shall be punished by a fine of not less than six hundred dollars nor more than seven hundred fifty dollars or by imprisonment for not more than one hundred eighty days or by both such fine and imprisonment.

1. Any person convicted of a violation of this section while driving any vehicle carrying passengers under eighteen years of age, any bus carrying passengers, any school bus or any vehicle carrying explosive substances or flammable liquids as a cargo or part of a cargo, shall, upon conviction of a first offense, be guilty of a class B misdemeanor, and shall, upon conviction of a second or subsequent offense committed within five years of the prior offense, be guilty of a class E felony.

2. Any person convicted of a violation of this section resulting in an accident which causes physical injury, as that term is defined pursuant to subdivision nine of section 10 of the penal law, serious physical injury, as that term is defined pursuant to subdivision ten of section 10.00 of the penal law, or death to another person, shall be guilty of a class E felony.

Nothing contained in this section shall be construed to prohibit or limit the prosecution of any violation, crime or other offense otherwise required or permitted by law.

**1171. Certain vehicles must stop at all railroad grade crossings.** (a) The driver of any school bus carrying passengers, of any school bus, of any motor vehicle with a gross weight rating of greater than ten thousand pounds that transports division 2.3

chlorine or is a cargo tank, whether loaded or empty, used to transport hazardous materials, as defined in section five hundred one-a of this chapter, of any motor vehicle required to be marked or placarded by either the United States department of transportation or the New York state department of transportation regulations or any vehicle carrying explosive substances or flammable liquids as a cargo or part of a cargo, of any crawler-type tractor, steam shovel, derrick, roller, or of any equipment or structure having a normal operating speed of ten or less miles per hour or a vertical body or load clearance of less than one-half inch per foot of the distance between any two adjacent axles or in any event of less than nine inches, measured above the level surface of a roadway, before crossing at grade any track or tracks of a railroad, shall stop such vehicle within fifty feet but not less than fifteen feet from the nearest rail of such railroad and while so stopped shall listen and look in both directions along such track for any approaching train, and for signals indicating the approach of a train, except as hereinafter provided, and shall not proceed until he can do so safely. After stopping as required herein and upon proceeding when it is safe to do so the driver of any said vehicle shall cross only in such gear of the vehicle that there will be no necessity for changing gears while traversing such crossing and the driver shall not shift gears while crossing the track or tracks.

(b) No stop need be made at any such crossing where a police officer or a traffic-control signal or sign directs traffic to proceed.

(c) Every motor vehicle used in commerce with a gross vehicle weight rating of greater than ten thousand pounds not subject to the requirements of subdivision (a) of this section shall upon approaching a railroad grade crossing, be driven at a rate of speed which will permit said motor vehicle to be stopped before reaching the nearest rail of such crossing, and shall not be driven upon or over such crossing until due caution has been taken to ascertain that the course is clear.

(d) In addition to the requirements of subdivisions (a) and (c) of this section, the driver of a commercial vehicle must check street-railway grade crossings within a business or residence district.

**§ 1172. Stop signs and yield signs.** (a) Except when directed to proceed by a police officer, every driver of a vehicle approaching a stop sign shall stop at a clearly marked stop line, but if none, then shall stop before entering the crosswalk on the near side of the intersection, or in the event there is no crosswalk, at the point nearest the intersecting roadway where the driver has a view of the approaching traffic on the intersecting roadway before entering the intersection and the right to proceed shall be subject to the provisions of section eleven hundred forty-two.

(b) The driver of a vehicle approaching a yield sign if required for safety to stop shall stop at a clearly marked stop line, but if none, then shall stop before entering the crosswalk on the near side of the intersection, or in the event there is no crosswalk, at the point nearest the intersecting roadway where the driver has a view of the approaching traffic on the intersecting roadway before entering the intersection and the right to proceed shall be subject to the provisions of section eleven hundred forty-two.



**Town of New Windsor**

555 Union Avenue  
New Windsor, NY 12553  
(845) 563-4611

**RECEIPT**  
**#699-2007**

09/13/2007

Vails Gate Business Center  
460 Getty Ave  
Clifton, NJ 07011

Received \$ 125.00 for Planning Board Fees, on 09/13/2007. Thank you for stopping by the Town Clerk's office.

As always, it is our pleasure to serve you.

**Deborah Green**  
Town Clerk

*PB# 06-32 approval.*

PLANNING BOARD  
TOWN OF NEW WINDSOR

AS OF: 09/13/2007

PAGE: 1

LISTING OF PLANNING BOARD FEES  
APPROVAL

FOR PROJECT NUMBER: 6-32  
NAME: VAILS GATE BUSINESS PARK PA2006-988  
APPLICANT: VAILS GATE BUSINESS CENTER LLC

--DATE--	DESCRIPTION-----	TRANS	--AMT-CHG	-AMT-PAID	--BAL-DUE
08/30/2007	APPROVAL FEE	CHG	125.00		
09/12/2007	REC. CK. # 011493	PAID		125.00	
		TOTAL:	125.00	125.00	0.00

PLANNING BOARD  
TOWN OF NEW WINDSOR

AS OF: 09/13/2007

PAGE: 1

LISTING OF PLANNING BOARD ACTIONS

STAGE:

STATUS [Open, Withd]  
A [Disap, Appr]

FOR PROJECT NUMBER: 6-32

NAME: VAILS GATE BUSINESS PARK PA2006-988

APPLICANT: VAILS GATE BUSINESS CENTER LLC

--DATE--	MEETING-PURPOSE-----	ACTION-TAKEN-----
09/12/2007	PLANS STAMPED	APPROVED
05/09/2007	P.B. APPEARANCE	APPROVE COND
12/13/2006	P.B. APPEARANCE . CORNWALL TO BE L.A. - . AND CORNWALL	RETURN SIMULTANEOUS SUBMISSIONS BETWEEN NW
10/04/2006	WORK SHOP	SUBMIT



# Town of New Windsor

555 Union Avenue  
New Windsor, New York 12553  
Telephone: (845) 563-4615  
Fax: (845) 563-4689

## OFFICE OF THE PLANNING BOARD

August 30, 2007

T.M. DePuy Engineering and Land Surveying, P.C.  
2656 Route 302  
Middletown, NY 10941

ATTN: THOMAS DEPUY, PE/LS

SUBJECT: VAILS GATE BUSINESS CENTER (06-32)

Dear Sir:

Please find attached printouts of fees due for subject project.

Please contact your client, the applicant, and ask that payment be submitted in separate checks, payable to the Town of New Windsor, as follows:

Check #1 – Approval Fee.....	\$	125.00
Check #2 – Amount due over Escrow Posted.....	\$	741.60

The plans have been signed by the Town of New Windsor Planning Board at this time and are ready for release upon receipt of the above checks.

If you have any questions in this regard, please contact my office.

Very truly yours,

\_\_\_\_\_  
Myra L. Mason, Secretary To The  
NEW WINDSOR PLANNING BOARD

MLM

PLANNING BOARD  
TOWN OF NEW WINDSOR

AS OF: 08/30/2007

PAGE: 1

LISTING OF PLANNING BOARD FEES  
APPROVAL

FOR PROJECT NUMBER: 6-32  
NAME: VAILS GATE BUSINESS PARK PA2006-988  
APPLICANT: VAILS GATE BUSINESS CENTER LLC

--DATE--	DESCRIPTION-----	TRANS	--AMT-CHG	-AMT-PAID	--BAL-DUE
08/30/2007	APPROVAL FEE	CHG	125.00		
		TOTAL:	125.00	0.00	125.00

PLANNING BOARD  
TOWN OF NEW WINDSOR

AS OF: 08/30/2007

PAGE: 1

LISTING OF PLANNING BOARD FEES  
ESCROW

FOR PROJECT NUMBER: 6-32  
NAME: VAILS GATE BUSINESS PARK PA2006-988  
APPLICANT: VAILS GATE BUSINESS CENTER LLC

--DATE--	DESCRIPTION-----	TRANS	--AMT-CHG	-AMT-PAID	--BAL-DUE
11/30/2006	REC. CK. #17074	PAID		750.00	
12/13/2006	P.B. MINUTES	CHG	70.00		
12/13/2006	CORDISCO - ATTY FEE	CHG	490.00		
04/25/2007	P.B. MINUTES	CHG	49.00		
05/09/2007	P.B. MINUTES	CHG	35.00		
08/30/2007	P.B. ENGINEER	CHG	847.60		
		TOTAL:	1491.60	750.00	741.60

Date		Received From/Paid To	Che#	ALL DATES		General		Bld		Trust Activity	
Entry #	Explanation		Rec#	Rcpts	Disbs	Fees	Inv#	Acc	Rcpts	Disbs	Balance
12132	TOWN OF NEW WINDSOR										
6044915	VAILS GATE BUSINESS PARK- SITE PLAN PB# 06-32										Resp Lawyer: JRL
Oct 20/2006	Town of New Windsor		024225	87.50							
51809	PMT - PAYMENT ON ACCOUNT										
Dec 12/2006	Lawyer: DRC 0.20 Hrs X 175.00					35.00	2216				
39771	REVIEW M EDSALL'S COMMENTS PB# 06-32										
Dec 13/2006	Lawyer: DRC 0.30 Hrs X 175.00					52.50	2216				
39860	ATTEND PLANNING BOARD MEETING PB# 06-32										
Jan 9/2007	Billing on Invoice 2216				0.00		2216				
44713	FEES 87.50										
Apr 25/2007	Lawyer: DRC 0.10 Hrs X 175.00					17.50	3551				
66632	REVIEW TOWN ENGINEER'S COMMENTS PB# 06-32										
Apr 25/2007	Lawyer: DRC 0.40 Hrs X 175.00					70.00	3551				
66633	ATTEND PLANNING BOARD MEETING PB# 06-32										
May 9/2007	Lawyer: DRC 0.10 Hrs X 175.00					17.50	3839				
69625	TELEPHONE CONFERENCE W/ M BABCOCK PB# 06-32										
May 9/2007	Lawyer: DRC 0.10 Hrs X 175.00					17.50	3839				
69626	REVIEW OCPD 239 REPORT PB# 06-32										
May 9/2007	Lawyer: DRC 0.80 Hrs X 175.00					140.00	3839				
69627	AGGREGATE TIME SPENT FOR SITE PLAN APPROVAL RESOLUTION PB# 06-32										
May 9/2007	Lawyer: DRC 0.50 Hrs X 175.00					87.50	3839				
69628	AGGREGATE TIME SPENT REVIEWING AND REVISING THE DRAFT LETTER TO THE PLANNING BOARD AND REVIEWING VARIOUS EMAILS PB# 06-32										
May 9/2007	Lawyer: DRC 0.10 Hrs X 175.00					17.50	3839				
69631	REVIEW LETTER FROM NYSDEC RE STORM WATER PB# 06-32										
May 9/2007	Lawyer: DRC 0.20 Hrs X 175.00					35.00	3839				
69632	ATTEND PLANNING BOARD MEETING PB# 06-32										
May 16/2007	Billing on Invoice 3551				0.00		3551				
69823	FEES 87.50										
May 25/2007	Town of New Windsor		010197	87.50							
72003	PMT - PAYMENT ON ACCOUNT										
Jun 12/2007	Billing on Invoice 3839				0.00		3839				
75379	FEES 315.00										
Jun 25/2007	Town of New Windsor		010452	315.00							
78638	PMT - PAYMENT ON ACCOUNT										

TOTALS	UNBILLED				= TOTAL	BILLED			BALANCES		
	CHE	+ RECOV	+ FEES	DISBS		+ FEES	+ TAX	- RECEIPTS	= A/R	TRUST	
PERIOD	0.00	0.00	0.00	0.00	0.00	0.00	490.00	0.00	490.00	0.00	0.00
END DATE	0.00	0.00	0.00	0.00	0.00	0.00	490.00	0.00	490.00	0.00	0.00

FIRM TOTAL	UNBILLED				= TOTAL	BILLED			BALANCES		
	CHE	+ RECOV	+ FEES	DISBS		+ FEES	+ TAX	RECEIPTS	= A/R	TRUST	
PERIOD	0.00	0.00	0.00	0.00	0.00	0.00	490.00	0.00	490.00	0.00	0.00
END DATE	0.00	0.00	0.00	0.00	0.00	0.00	490.00	0.00	490.00	0.00	0.00

REPORT SELECTIONS - Client Ledger

Layout Template Default  
 Requested by Jennifer Schneider  
 Finished Wednesday, August 29, 2007 at 03:34:05 PM  
 Ver 8.20c  
 Matters 6044915  
 Clients All  
 Major Clients All  
 Client Intro Lawyer All  
 Matter Intro Lawyer All  
 Responsible Lawyer All  
 Assigned Lawyer All  
 Type of Law All  
 Select From Active, Inactive Matters  
 Matters Sort by Default  
 New Page for Each Lawyer No  
 New Page for Each Matter No  
 No Activity Date Dec 31/2199  
 Firm Totals Only No  
 Totals Only No  
 Entries Shown - Billed Only No  
 Entries Shown - Disbursements Yes  
 Entries Shown - Receipts Yes  
 Entries Shown - Time or Fees Yes  
 Entries Shown - Trust Yes  
 Incl. Matters with Retainer Bal No  
 Incl. Matters with Neg Unbld Disb No  
 Trust Account All  
 Working Lawyer All

Client Ledger  
ALL DATES

Date	Received From/Paid To	Chef	General	Bld	Trust Activity			Balance
Entry #	Explanation	Rec#	Disbs	Inv#	Acc	Rcpts	Disbs	
	Include Corrected Entries		No					
	Show Check # on Paid Payables		No					
	Show Client Address		No					
	Consolidate Payments		No					
	Show Trust Summary by Account		No					

CHRONOLOGICAL JOB STATUS REPORT

JOB: 87-56

NEW WINDSOR PLANNING BOARD (Chargeable to Applicant)

CLIENT: NEWWIN - TOWN OF NEW WINDSOR

TASK: 6- 32

FOR ALL WORK ON FILE:

TASK-NO	REC	--DATE--	TRAN	EMPL	ACT DESCRIPTION	RATE	HRS.	TIME	EXP.	-----DOLLARS-----	
										BILLED	BALANCE
6-32	308517	10/04/06	TIME	MJE	WS KESSLER TACKETT	115.00	0.60	69.00			
6-32	315919	12/05/06	TIME	MJE	MC VG BUS PK W/GA	115.00	0.10	11.50			
6-32	315910	12/06/06	TIME	MJE	MR VG BUS PRK SITE PLAN	115.00	0.50	57.50			
6-32	315913	12/07/06	TIME	MJE	MC VG BUS TRAFFIC ISSUE	115.00	0.30	34.50			
6-32	315915	12/07/06	TIME	MJE	MR VG BUS PK SITE PLAN	115.00	0.40	46.00			
6-32	315938	12/07/06	TIME	MJE	MC VG BUS PK W/GA	115.00	0.20	23.00			
6-32	317837	12/22/06	TIME	MJE	MC VG BUS PK W/MICHELLE	115.00	0.20	23.00			
								264.50			
6-32	317168	12/22/06			BILL 06-3206						-172.50
											-172.50
6-32	368712				PD/CR 06-3206 PD 01/09/07			172.50			
6-32	327556	03/01/07	TIME	MJE	AA NYS DOT REF VG BUS PR	119.00	0.40	47.60			
								47.60			
6-32	328511	03/15/07			BILL 07-844						-139.60
											-139.60
6-32	369422				PD/CR 07-844 PD 03/28/07			139.60			
6-32	334601	04/19/07	TIME	MJE	MC VG BUS PK MEMO	119.00	0.70	83.30			
6-32	335592	04/23/07	TIME	MJE	MR VG BUS PK COMMENTS	119.00	0.20	23.80			
6-32	335602	04/24/07	TIME	MJE	MM VG BUS PK	119.00	0.30	35.70			
6-32	335610	04/25/07	TIME	MJE	MC VG BUS PK W/GA	119.00	0.10	11.90			
6-32	335611	04/26/07	TIME	MJE	MC GA DISC VG TRAFF ISS	119.00	0.40	47.60			
6-32	337010	04/30/07	TIME	MJE	MC GREATLY:VG BUS PK TRA	119.00	0.30	35.70			
6-32	337030	05/04/07	TIME	MJE	MC VG BUS PK W/MM	119.00	0.30	35.70			
6-32	337031	05/04/07	TIME	MJE	MC TC RET MICHELLE B:VG	119.00	0.30	35.70			
6-32	338203	05/08/07	TIME	MJE	MR VG BUS PK S/P	119.00	0.40	47.60			
6-32	338210	05/08/07	TIME	MJE	PM MTG GA/LEAD R/VW PROJ	119.00	0.20	23.80			
6-32	338216	05/08/07	TIME	MJE	MC OGDY REV & BMC MM	119.00	0.30	35.70			
6-32	338450	05/09/07	TIME	MJE	MM VG BUS PK APPD	0.00	0.10	0.00			
6-32	338700	05/09/07	TIME	END	MR VAILS GATE BUS PRK	119.00	0.30	35.70			
								452.20			
6-32	339784	05/23/07			BILL 07-1353						-452.20
											-452.20
6-32	370470				PD/CR 07-1353 PD 06/11/07			452.20			
6-32	349395	07/20/07	TIME	MJE	MC VG BUS PK C/O PLAN	119.00	0.50	59.50			
6-32	349396	07/20/07	TIME	MJE	MC TC/LM DEPUT:APP	119.00	0.20	23.80			
								847.60			
									0.00		-764.30
											83.30
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									0.00		-764.30
											83.30

GRAND TOTAL

847.60

0.00

-764.30

83.30

REGULAR ITEMS:

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VAILS\_GATE\_BUSINESS\_PARK\_(06-32)

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MR. ARGENIO: Vails Gate Business Park. This application proposes the redevelopment of the former Tarkett facility with general manufacturing, industrial processing and self-storage. The plan was previously reviewed at the 13 December, 2006 and 25 April, 2007 planning board meeting. To refresh the board's memory, this is the one that Cornwall took lead agency on and Mr. Wolinsky is here to represent this. Give us a brief summary of where you are and we certainly have comments here from Mark.

MR. WOLINSKY: This is the existing Tarkett manufacturing facility which has applied for an adaptive reuse which will break it down into some light manufacturing, some warehousing and storage. As you point out, Mr. Chairman, that has been extensively reviewed by the Town of Cornwall and as most of the property is in that town and recently the Town of Cornwall has issued a negative dec and its approval for the project. I understand that the project was before this board last month, the approvals from Cornwall have occurred in the intervening time period and we're here this evening before you hopefully to finish up the process, it has been referred to the County, the County signed off in February with an approval letter.

MR. ARGENIO: I have in my hand the negative dec from Cornwall which came through I think today.

MR. CORDISCO: Well, it was adopted Monday night.

MR. ARGENIO: I received it today on my e-mail so comment 2 of Mark's comments, plans have received significant additions and corrections as part of the ongoing Town of Cornwall Planning Board review, that board adopted SEQRA negative dec resolution, okay, I

didn't realize Mark got that in his comments, okay.

MR. DENEGA: Revised that as of this afternoon.

MR. CORDISCO: If I could expand on additional procedural status of this, if you recall this planning board waived public hearing with a condition that the Town of Cornwall public hearing when those notices went out--

MR. ARGENIO: New Windsor residents are notified.

MR. CORDISCO: That's correct. That was done, there was a public hearing before the Town of Cornwall and as Mr. Wolinsky points out, the plans were referred to the County Planning Department and the County Planning Department picked up on the fact that it was across municipal borders and the report that came back referenced the fact that it was both in the Town of Cornwall and Town of New Windsor. And so I think that the requirements of the General Municipal Law have been satisfied in this regard.

MR. ARGENIO: We don't have to submit to the County?

MR. CORDISCO: No, you do not.

MR. ARGENIO: At least something makes sense tonight. I don't have any problem with this application. I want to hear from the other board members. My main concern was traffic and Phil Greely of John Collins was here at the last meeting and he certainly allayed the concerns that I had but again, Neil or Howard?

MR. SCHLESINGER: There was an issue with the sight distance by the Thruway landscaping to the left to the west of the driveway.

MR. WOLINSKY: That's right, the traffic study recommended that an area, a triangular area be cleared

of vegetation, that's been incorporated into the approval resolutions and we would certainly have no problem with it being a condition.

MR. SCHLESINGER: Matters so that the landscaping can't grow back, it's supposed to be cleared so I don't know how it's going to be addressed.

MR. WOLINSKY: It will have to be maintained.

MR. ARGENIO: So Mr. Wolinsky that area when you visually look to the left is going to be cut down, when I say cut down, the grade will be lowered, is that correct?

MR. WOLINSKY: I'm not sure, I'm not a hundred percent sure.

MR. ARGENIO: I'm almost positive, let's just make a little note of this, just check this, the topo on the plan to the left of that entrance showed the grades being changed there last time somebody in this room up on this dais probed that question quite thoroughly last time as I recall.

MR. VAN LEEUWEN: I did.

MR. ARGENIO: Yes.

MR. VAN LEEUWEN: Cause that's been a problem there as long as I can remember, if they don't cut that hill down the little knoll.

MR. GALLAGHER: Mentioned regrading it.

MR. ARGENIO: Yes, he did mention that.

MR. CORDISCO: Yes, I believe that those changes have been shown on the plans.

May 9, 2007

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MR. WOLINSKY: Yeah, the negative dec refers to removal of vegetation but I don't have the actual grading plan with me.

MR. CORDISCO: The written resolution that I have prepared for this board's consideration requires that they post bonds or other form of security to ensure that the off-site improvements are made.

MR. ARGENIO: So if it's not done we have a bond in place to compel them to do it and it's in the minutes now it's certainly in the minutes.

MR. WOLINSKY: That's fine with us, Mr. Chairman.

MR. ARGENIO: Very good, Mr. Wolinsky. Anybody else? Dominic, do we have to, we have to vote to accept the negative dec, is that correct?

MR. CORDISCO: You should vote to rely on the Town of Cornwall's negative declaration cause they did the coordinated review, as you recall, so I think you don't have to adopt one, you can adopt the Town of Cornwall's if you so choose.

MR. VAN LEEUWEN: I make that motion.

MR. SCHLESINGER: Second it.

MR. ARGENIO: Motion has been made and seconded that the Town of New Windsor Planning Board adopt the negative dec declared by Cornwall. If there's no further discussion, roll call.

ROLL CALL

MR. SCHLESINGER	AYE
MR. BROWN	AYE
MR. GALLAGHER	AYE
MR. VAN LEEUWEN	AYE

May 9, 2007

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MR. ARGENIO            AYE

MR. ARGENIO:    What else do we need to do?

MR. CORDISCO:    Grant site plan approval.

MR. ARGENIO:    Even though they're--

MR. VAN LEEUWEN:    So moved.

MR. SCHLESINGER:    Second it.

MR. ARGENIO:    motion has been made and seconded that  
the Town of New Windsor Planning Board offer final  
approval to the Vails Gate Business Park former  
Tarkett.    No further discussion, roll call.

ROLL CALL

MR. SCHLESINGER      AYE

MR. BROWN            AYE

MR. GALLAGHER        AYE

MR. VAN LEEUWEN      AYE

MR. ARGENIO            AYE



**McGOEY, HAUSER and EDSALL  
CONSULTING ENGINEERS P.C.**

RICHARD D. MCGOEY, P.E. (NY & PA)  
WILLIAM J. HAUSER, P.E. (NY & NJ)  
MARK J. EDSALL, P.E. (NY, NJ & PA)  
JAMES M. FARR, P.E. (NY & PA)

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**TOWN OF NEW WINDSOR**  
**PLANNING BOARD**  
**REVIEW COMMENTS**

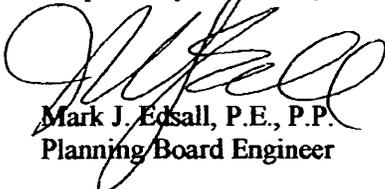
**PROJECT NAME:** VAILES GATE BUSINESS PARK  
(REDEVELOPMENT OF TARKETT SITE)  
**PROJECT LOCATION:** OFF NYS ROUTE 94  
SECTION 69 – BLOCK 4 – LOT 3  
**PROJECT NUMBER:** 06-32  
**DATE:** 9 MAY 2007  
**DESCRIPTION:** THE APPLICATION PROPOSES THE REDEVELOPMENT OF THE FORMER TARKETT FACILITY (APPROX. 36 ACRES TOTAL LAND AREA) WITH GENERAL MANUFACTURING, INDUSTRIAL PROCESSING, AND SELF-STORAGE. THE PLAN WAS PREVIOUSLY REVIEWED AT THE 13 DECEMBER 2006 AND 25 APRIL 2007 PLANNING BOARD MEETINGS.

1. The project spans the Town of Cornwall – Town of New Windsor line, and will be the subject of a concurrent review by the Cornwall Planning Board (Cornwall Application 06-17).

The property is located in the C Zoning District of the Town. The use proposed in the New Windsor portion of the site is Mini-Warehouse, which is use A-6 of the zone. The bulk table has been corrected as was previously requested.

2. The plans have received significant additions and corrections as part of the ongoing Town of Cornwall Planning Board review. That Board adopted a SEQRA “neg dec” resolution and a final approval resolution at their 5-7-07 meeting. *If this Board agrees with the adopted negative declaration, they may wish to formally concur with such declaration.*
3. All my requested corrections and additions are included on these plans. The approval resolution should include the posting of an off-site improvement bond, on-site bond estimate, and payment of all fees.

Respectfully Submitted,



Mark J. Edsall, P.E., P.P.  
Planning Board Engineer

MJE/st NW06-32-09May07.doc

**REGIONAL OFFICES**

• 111 WHEATFIELD DRIVE – SUITE ONE • MILFORD, PENNSYLVANIA 18337 • 570-296-2765 •  
• 540 BROADWAY • MONTICELLO, NEW YORK 12701 • 845-794-3399 •

VAILS\_GATE\_BUSINESS\_PARK\_(06-32)

MR. ARGENIO: Vails Gate Business Park, this is for discussion. Mr. Phil Greely from John Collins is here and Tom Depuy for whatever reason, we'll find out in a minute. This is the Tarkett site, guys, most of this site is in Cornwall and if I could just jar everybody's memory a little bit. Essentially, what we said on this let the Town of Cornwall be lead agency, let them review this thing because the only thing we're concerned about is traffic. So what we told them was go do your bit with the Town of Cornwall, go through your approval process. When you start to get close, please come back and see us and let us know where traffic's at and give us an update and let us know how you're doing but don't do it after your final approval so these folks are here.

MR. VAN LEEUWEN: You've got to have our approval.

MR. ARGENIO: These folks are here so Phil, I'll give it to you.

MR. GREELY: Mr. Chairman, Phillip Greely, John Collins Engineers. We had prepared a traffic study for the reuse of the property, the Town of New Windsor and Cornwall both forwarded the information to the New York State Department of Transportation because Tarkett Drive of course intersects with Route 94. DOT responded back, they reviewed the application, there were certainly improvements that we proposed at the entrance to Tarkett Drive as you know intersects with 94, there's a separate left turn lane on 94 for vehicles turning into the property, there are essentially three improvements that we recommended. Number one, was some additional paving within the right-of-way to improve the ability to turn off 94 into Tarkett Drive, the pavement is in poor condition right there. Number two is to improve the sight lines, they're as you exit from Tarkett Drive under the

Thruway there's a slope and vegetation in that area which we have a detail which shows the regrading and the clearing to improve that sight line. And then the third point is as you're heading eastbound on 94 just before the Thruway structure there's a warning sign that's in place, it was there when Tarkett was operating.

MR. ARGENIO: What is it warning for?

MR. GREELY: It's indicating the access drive ahead, it's a standard black yellow warning sign with flashing beacons, plant entrance ahead, that has to be upgraded to current standards, you know, one of the beacons is functioning, the other one isn't today so those were identified as improvements. DOT reviewed that, conceptually approved that there was a letter I think sent back to Mark from DOT, we have to get of course the work permit to do that. In terms of the traffic study and what was looked at here we looked at the former use of the property in terms of traffic generation, we did not have actual counts of the facility but based on the square footage and the shift operation the proposed reuse here will actually spread the traffic out a little bit more, you won't get that real peaking, you know, that you would have with the former use with the factory there so that's a benefit from a traffic standpoint. There would be slightly less peak hour traffic generation, you know, from this facility and that's pretty much where we are, what we've looked at and where we are in the process with the DOT and Town of Cornwall.

MR. ARGENIO: How close are you with Cornwall?

MR. GREELY: I think we're hoping at the next meeting to get our approvals from them. We've gone through several meetings and addressed all their concerns, including some site plan concerns. I think we're pretty much up to date on addressing all the site plan

issues within the Town of Cornwall.

MR. VAN LEEUWEN: For years and I've been here for a long time, okay, we've had trouble, a lot of trouble with sight distance there on the left-hand side as coming out of Tarkett.

MR. GREELY: Right in this area.

MR. VAN LEEUWEN: What do you propose?

MR. GREELY: That's where we're proposing to grade back the slope as you're exiting Tarkett Drive, you look over the rail tracks, there's a slope here and there's, it's not too bad right now in terms of the vegetation that's there but there's brush in there so in a matter of a couple weeks it's going to be worse than it is now.

MR. VAN LEEUWEN: Because the people that live there fought us tooth and nail.

MR. GREELY: Well, the area within the right-of-way of the highway there's an area there that we can grade back, that's what we're proposing within the right-of-way to grade that back and to cut the vegetation that's there because it's just brush but it impedes the sight lines.

MR. VAN LEEUWEN: What are you going to put in place?

MR. GREELY: Well, we have to show the detail plan to DOT but as I said in concept they have agreed with it based on the material that's there, some of which is rock, we'll probably try to put something, crown vetch or something that's low cover that stabilizes the slope but does not impede the sight lines.

MR. ARGENIO: Nothing grows through crown vetch.

MR. VAN LEEUWEN: We've had problems for years, we've tried to rectify, there's a woman living there, she wouldn't budge but you do have it rectified.

MR. ARGENIO: Let me ask you a direct question. Are you going to increase traffic to a level higher than what the traffic level was when Tarkett was operational?

MR. GREELY: No, as part of our study and some of the followup information Mark asked for we actually prepared a table that showed a comparison and we on a peak hour basis we'll actually have less traffic, not a lot less but there will be, right now, of course, you have no traffic from this going through the Five Corners but in comparison if you re-occupied and Tarkett came back in there we would have less peak hour traffic, I think we're probably within 10, 15 cars of that but still less than what would be there before. In round numbers to give you an idea, you know, in the afternoon rush hour you have well over 3,000 vehicles passing through the Five Corners, okay, we're looking at generating somewhere around 80 to 90 vehicles that would go through the intersection so what's that, it's about probably around two to three percent but again it would be less than what Tarkett would have generated at its full occupation of this facility.

MR. VAN LEEUWEN: At 5 o'clock at night?

MR. GREELY: Right.

MR. VAN LEEUWEN: What's going to go into this building, do you have any idea?

MR. GREELY: There are some uses that have been already identified, basically light manufacturing, warehouse type uses.

MR. VAN LEEUWEN: How big is the building?

MR. GREELY: It's 225,000 square feet, I think 224.

MR. DEPUY: Actually, three buildings.

MR. ARGENIO: Mark or I should say Dominic, they need our final approval on this before they can go ahead with this project, is that correct?

MR. CORDISCO: That's correct.

MR. ARGENIO: Formal approval?

MR. CORDISCO: That's correct but--

MR. SCHLESINGER: Being that Cornwall's lead agency, whose fire approval do they get?

MR. VAN LEEUWEN: They have to have ours first.

MR. EDSALL: I'll tell that because it spans the line early in the process I brought Ken Schermerhorn from New Windsor Fire Office, Inspector's Office together with Gary Vinson and they worked together to solve all the issues so that's all been taken care of early on.

MR. CORDISCO: Because the Cornwall Planning Board is lead agency, they have directed at this point that resolutions be prepared for their May meeting but they haven't formally adopted them yet so there's been no negative dec at this point, since they're lead agency, this board can't act until SEQRA's concluded. This board could if it's satisfied with the answers tonight direct me to prepare resolutions for its next meeting and hopefully at that point Cornwall will have acted and this board can act.

MR. VAN LEEUWEN: I thought we had to act first then they act?

MR. ARGENIO: No, they're lead agency.

MR. VAN LEEUWEN: You're right, sorry.

MR. ARGENIO: I will be frank with you, I want to think about the traffic thing a little bit, that's my opinion but there's four other people here, I'm not twisted up about it, Phil, but I just would like to think about it, that's all. Certainly I have for the record I have done work professionally in the past with Phil Greely and he's probably one of the most competent traffic engineers in southern New York based upon experience but I would like just the opportunity to think about it a little bit. Anybody else have any thoughts on this?

MR. VAN LEEUWEN: I agree you.

MR. SCHLESINGER: Come up with a significant evaluation, I'd probably want to know how many employees Tarkett had, what their shifts were, if you're going to compare apples to oranges you want to have some information.

MR. ARGENIO: Neil, as Phil stated he did prepare a comparison and give it to Mark and gave it to Mark and I'm sure Mark has reviewed it and quite honestly that's what I want to think about.

MR. SCHLESINGER: I thought he said he didn't have figures.

MR. GREELY: What I indicated was we did not have exact traffic counts because when we were brought on board it was already vacant so what we did was based on the historical information we had in terms of employees, size of buildings and published data published by the Institute of Transportation Engineers came up with the numbers so the point was that it wasn't actual counts.

MR. SCHLESINGER: You don't have traffic counts but

April 25, 2007

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have an idea of what is going on?

MR. GREELY: Traffic projection based on what was there and that category of use, that type of factory operation and that's how we do it and that's the best method you have.

MR. VAN LEEUWEN: When Tarkett was in this and it was 5 o'clock it was a zoo but this way the traffic comes out more slowly.

MR. EDSALL: It's distributed.

MR. VAN LEEUWEN: So you don't get it all at one time.

MR. ARGENIO: I don't have anything else. Anybody else have anything else? Thank you.

MR. DEPUY: We had some original data that indicated 110 people at the maximum shift which was given to Phil at the time so that, you know, the information that he has is, you know, from the actual.

MR. ARGENIO: Very good.

MR. VAN LEEUWEN: We've had a lot of accidents there too.

MR. ARGENIO: Thanks, guys.

**NEGATIVE DECLARATION**  
**VAILS GATE BUSINESS PARK - RE-USE OF FORMER TARKETT**  
**MANUFACTURING PLANT**  
**LEAD AGENCY,**  
**TOWN OF CORNWALL PLANNING BOARD, COUNTY OF ORANGE**

Please take notice that, according to the provisions of Article 8 of the Environmental Conservation Law and the New York Code of Rules and Regulations Part 617.7(d), the Town of Cornwall Planning Board has adopted a Negative Declaration for the project named below. The Planning Board is serving as Lead Agency for Coordinated Review of this Type I Action, having circulated notice of its intent to potentially Involved Agencies on November 20, 2006, along with the Full EAF and other project information, and having received no objections within 30 days thereof.

**Name of Project: Vails Gate Site Plan**

**Action Type: Type I Action**

**Action Type: Type I Action**

**Location: 1215 Route 94, partly in Town of New Windsor**

**Zoning District: PIO (Planned Industrial Office) Town of Cornwall**

**C (Design Shopping) Town of New Windsor**

**Tax Map Parcels: Section 4, Block 3, Lot 1.1 (Town of Cornwall)**  
**Section 69, Block 4, Lot 3 (Town of New Windsor)**

**Summary of Action:**

The site in question is the former Tarkett floor covering manufacturing facility, located on a total of just under 36 acres of land, 29.1 of which are situated in the Town of Cornwall and 6.8 of which are in the Town of New Windsor. The site takes its sole access from NY State Route 94 in the Town of New Windsor, east of the NY State Thruway and east of an unused rail line that is owned by others. The facility consists of three existing buildings: (1) Building 1, the 224,178 square-foot manufacturing facility with eleven loading docks, lying wholly in Cornwall; (2) Building 2, a 14,328 square foot building that is divided by the municipal boundary, and (3) Building 3, an 18,388 square foot building lying wholly in Cornwall. There is an existing large, partially paved parking area that is divided by the municipal boundary, and internal access and circulation drives. The site is served by municipal water and sewer. A portion of the site consists of state Freshwater Wetland CO-11 and its regulated 100-foot Adjacent Area, and there are also areas of federal jurisdictional wetland on the site, the boundaries of which were located on the field and appear on the surveyed plan.

The applicant is proposing to re-use the site for a mix of uses including warehousing, general manufacturing, and self-storage. The intent is to divide the

existing structures for the use of multiple tenants for such uses. The applicant is seeking land use approvals for the re-development of the site as a coordinated entity, because the site has functioned and must continue to function as a single entity due to the existing access and other site limitations.

The Planning Board designated this action as Type I due to the proximity of the historic 1755 Edmonston House in the Town of New Windsor.

**Reasons Supporting Negative Declaration:**

Based on its consideration of the available information, the Lead Agency Town of Cornwall Planning Board finds there would be no significant adverse environmental effects associated with granting conditioned site plan approval to this site plan approving the re-use of the total 246,210 square feet of the former Tarkett manufacturing plant for a mix of 37.4% light manufacturing (for a total of 92,176 square feet light manufacturing), 61.1% warehousing (for a total of 150,390 square feet of warehousing) and 1.5% mini-storage (for a total of 3,644 square feet of mini-storage). The Town of Cornwall Planning Board acknowledges that the overall site functions as a single cohesive site and its own land use approvals are conditioned on action by the Town of New Windsor Planning Board, in addition to the receipt of approvals by other permit-granting agencies. The Lead Agency's SEQR Negative Declaration is based on the following reasons:

**Traffic and Transportation**

▪ *Layout, Access:*

The site's sole road access is already existing and lies within an 58.3-foot strip of frontage on the south side of NYS Route 94 located in the Town of New Windsor. At the unsignalized site driveway, the highway consists of one travel lane in each direction, with an additional center turning lane at the westbound approach, serving the site's driveway and adjoining driveways. The existing driveway intersects with the highway just east of a railroad grade crossing, which itself is located east of the NYS Thruway overpass over Route 94. The site is located west of the complex signalized intersection of NYS Routes 32, 94, and 300.

▪ *Traffic Study:*

The applicant conducted a traffic study to evaluate the potential traffic impacts of the site re-development. Data was collected on existing "No-Build" year 2006 traffic conditions for the site driveway for existing weekday peak hour conditions, and future conditions were projected to the "Build" year 2009 using a 3% growth factor to account for general traffic growth. Traffic projections from other large pending projects in the area, such as Chestnut Woods and Cornwall Commons were included. Historic traffic data were also referenced.

Site-generated traffic volumes were projected to be 116 vehicles entering and 24 exiting the site during the AM peak hour, with 28 entering and 105 exiting during the PM peak hour. This projection was based on the mix of projected land uses within the site. The traffic study projected that most of the traffic (70%) arriving to and departing from the site would be towards the northeast.

The traffic study showed that the existing operating Levels of Service (LOS A) for the Route 94 intersection at existing site driveway for both AM and PM peak hours for Westbound turning movements were LOS "A", and future projected LOSs both with and without the project were unchanged. Existing northbound turning movements at this same intersection were LOS "B" for both AM and PM peak hours. Future 2009 LOSs without the project remained unchanged, but with the project, the AM peak hour LOS declined to "C" while the PM peak declined to LOS "D". Although the future northbound turning movements with the project will experience increased delays in both morning and evening peak hours, neither reaches a level of significance that would require mitigation.

- *Pedestrian Traffic*

No off-site pedestrian traffic is anticipated at the site, since the site location is already industrial/commercial in nature, and the proposed re-uses of the site are consistent with that non-pedestrian-oriented character. There is no pedestrian network provided along the nearby state highways in the Town of New Windsor. The only pedestrian provision incorporated onto the plan is an internal cross-connection between the large on-site parking area leading towards the largest building on the site.

- *Traffic Issues and Concerns*

The traffic study had incorporated several recommendations, including:

1. completion of certain intersection-related improvements: the need to clear existing vegetation be cleared within the south side of the Route 94 NYSDOT right of way looking west, in order to improve existing sight lines at the driveway, along with other improvements such as pavement markings and shoulder improvements, as well as re-activating and upgrading the warning sign and beacon on Route 94 west of the access drive. A conceptual improvement plan was provided showing these conceptual improvements; all these improvements are located in the Town of New Windsor and are under the jurisdiction of NYS DOT. Completion of this work will provide adequate sight lines at the existing driveway.
2. the study further recommended that if the existing rail siding were no longer active, the applicant should pursue an "exempt" posting. Although the CSX, the owner of the inactive rail siding, will not remove the siding, it still may be possible to post the rail crossing as "exempt" from the typical stopping requirements for school buses.

3. review of internal site plan layout with municipal emergency services for internal cross connections.

- ***Mitigation Measures***

1. DOT has registered no objections to the Conceptual Improvement Plan prepared for the site drive/intersection improvements. These are an integral part of the project plans and will be coordinated with DOT's permit approval requirement through the Town of New Windsor Planning Board as part of the land use approval process in the Town of New Windsor.
2. The plan has been reviewed by municipal emergency services, and the plan is accessible to the same with no objections.

Based on the preceding facts, the Lead Agency finds that the action will result in no significant harmful traffic, traffic safety or other transportation impacts.

### **Water Resources**

With respect to potable water resources, the site is served by existing municipal water supplies and sanitary sewer service, and the level of consumption that would be expected by the proposed re-use would not be expected to be significantly great. Warehousing and mini-warehouse uses consume incidental if any water or sewer, and light manufacturing uses are not characterized by significant water or sewer consumption. In any case, if the site specific interior uses in the Town of Cornwall, other than the portion of the mini-warehouse building that is divided by the municipal boundary, should exceed the use thresholds approved by the Planning Board and based on such use, require greater water or sewer consumption than anticipated in this Negative Declaration, the Town of Cornwall Planning Board has the opportunity to evaluate the specific individual future use through amended site plan approval. The only building uses in the Town of New Windsor are a portion of a mini-storage use, which consumes negligible water or sewer resources.

Fire hydrants are shown in appropriate locations throughout the site.

- ***Surface Water Drainage & Flooding- Issues, Impacts, Mitigation Measures***

The site contains both State and Federal jurisdictional wetlands. A portion of State Freshwater Wetland CO-11 extends into the site on both its east and western sides in the Town of Cornwall. The site plan proposes no new activities that would involve disturbance of wetland CO-11, but some site improvements on the west end of the site involve disturbance in the regulated 100-foot Adjacent Area. Strips of additional pavement needed for improved site circulation and emergency services access, convenience parking and truck access to various portions of the manufacturing facility (Building 1) being internally divided, and a portion of two proposed stormwater management ponds are proposed to be located in the

regulated buffer area for CO-11 and will require DEC permit. Because the existing developed site already lies partially in the buffer of CO-11, with even a small portion of the existing manufacturing Building 1 and Building 3 lying in the buffer, a certain amount of buffer disturbance is unavoidable. Because the site is no longer able to be used for a single large manufacturer with a large workforce, the Town of Cornwall Planning Board required additional parking spaces to be provided near to the buildings, recognizing that in the absence of such convenience parking spaces, future site users might otherwise have the potential to impede emergency services and truck circulation<sup>1</sup>. The two proposed stormwater management ponds are located as far outside the buffer area as possible, but the ability to site these is limited by other existing features such as existing rail sidings and the wetland itself. In any case, the Lead Agency finds that the provision of the stormwater management ponds will help to protect surface water quality, improving the quality of surface water runoff over existing site runoff conditions. In balance, then, the Lead Agency finds that the proposed action would not create any significant harmful impact on surface water resources.

Federal jurisdictional wetlands also exist on the site, and these have been identified in the field by Mike Nowicki on July 21, 2006. No disturbances to the federal jurisdictional wetlands are indicated; therefore no impact is expected in this regard.

As noted above, stormwater management plans are incorporated into the site plan and will incorporate "Best Management Practices" as recommended by the New York State Department of Environmental Conservation as indicated in their Stormwater Management Design Manual. These will require review and approval by DEC. The project plans will not result in any significant increase in impervious area, and in fact overall the plan will reduce the existing impervious area on the site due to existing impervious surfaces that are being removed. Therefore the plan will not cause or exacerbate any drainage or flooding problems, nor will it change any existing drainage patterns.

▪ ***Other Water Quality Issues - Test Wells, Remedial Investigations- Issues, Impacts, Mitigation Measures***

The site is the subject of on-going Remedial Field Investigations (RFI) stemming from activities carried on under the previous site use. The site was used for manufacturing floor coverings, and there were areas used for things such as storage of drums or underground tanks on the site. The Lead Agency has been informed that the on-going RFI, and any remedial measures that must be undertaken as a result of that study, is the entirely the responsibility of the previous owner and this study is proceeding under the jurisdiction of the

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<sup>1</sup> The reader should refer to the section on *Planning & Zoning* in this document for further evaluation of the parking issue.

NYSDEC as a separate action that was underway prior to this land use application being made to the Towns of Cornwall and New Windsor Planning Boards. Because the RFI and any subsequent remedial measures are the responsibility of the previous owner and are under the jurisdiction of the NYSDEC, the Lead Agency's SEQR review role in regard to this site activity was necessarily limited to the consideration of those Areas of Concern that might be affected by the proposed site plan. To that purpose, the Lead Agency evaluated an overlay of the RFI Areas of Concern on the proposed site plan, and was able to determine that the any new exterior site elements that are required on current site plan creates no potential conflicts with any of the Areas of Concern. This conclusion of no impact applies to the entire site, both in the Town of Cornwall and the Town of New Windsor. Where the RFI plan shows monitoring wells located in areas that will be subject to traffic circulation, they will be located in a reinforced, capped pit that is accessible for test purposes. No further consideration is needed in this subject area.

#### **Vegetation, Wildlife, Land Disturbance**

As noted in the project description, the action involves the re-use of an existing industrial facility, and there will be limited land disturbance associated with this re-use. The new land disturbance that will be created relates to the need to provide for full truck and emergency services circulation on both sides of the manufacturing plant, to create some convenience parking and access to the buildings and to create stormwater management ponds in compliance with current regulatory requirements. Virtually all of the areas that are being altered pursuant to the site plan have been historically disturbed, having either been graveled circulation surfaces, or having been the site of demolished railroad sidings, motorhouse and accessory structures, or purposes. Any currently vegetated areas that are being disturbed are adjacent to existing buildings or at the edge of currently paved or disturbed areas. There is no vegetation of wildlife habitat of any significance present on the site, confirmed by DEC inventory. Accordingly the Lead Agency concludes that the action will result in no significant harmful impacts either to vegetation, wildlife, or land disturbance.

Vegetation being cleared in the Town of New Windsor along Route 94 will benefit traffic safety by protecting sight distance at the site driveway. This will be required by NYSDOT for its permit approval and this improvement is incorporated in the plans.

#### **Planning and Zoning, Community Character**

The site is divided by the Town of Cornwall/Town of New Windsor municipal boundary and includes land within two districts, PIO (Planned Industrial-Office)

in Cornwall and C (Design Shopping) in New Windsor. The site consists of approximately 35.9 acres of land, 29.1 of which are situated in the Town of Cornwall and 6.8 of which are in the Town of New Windsor. The character of the site area is primarily industrial and commercial, consistent with the character of the existing developed site.

The applicant is seeking approval for a change of use from the previously existing general manufacturing use, which constitutes a special permit use in the Town of Cornwall. The existing site development already exceeds Cornwall's zoning requirements development coverage limits for the PIO district, but the proposed plan does not increase the degree of non-conformity due to the amount of impervious area that is being removed elsewhere on the site. The bulk requirements for the proposed mix of uses are identical with the current bulk requirements applying to the existing use. The Lead Agency understands that the site use complies with the Town of New Windsor zoning requirements; in any case, both Planning Board approvals must be contingent upon approval of all required agency approvals and permits.

Based on the size of the existing buildings, and the way that the Cornwall Code establishes parking requirements for certain land uses, the existing number of parking spaces falls short of the Town of Cornwall's parking requirements, though the existing parking spaces have been adequate to meet the needs of the previous user's employee needs at its maximum shift. Based on the mix of uses proposed at the site, the applicant has shown that the number of required spaces is actually lower than the number of currently existing spaces; therefore, no additional parking spaces are required. However, the Town of Cornwall Planning Board required that the plan be revised to re-locate a number of parking spaces closer to the buildings served. The existing parking spaces are located in a single centralized area that is remote from the buildings it serves. While this existing layout was suitable for a single large employer, it is not well suited for multiple different new users of the same building square footage. Because of the proposed shift in usage, with the building proposed to be divided for a mix of users, the Cornwall Planning Board determined that it was necessary for the various users to be able to park convenient to their businesses, in order to avoid the likelihood of parked cars obstructing fire truck access within the site. The revised plan shows an appropriate re-allocation of parking spaces to meet the needs of the new site uses. The plan also includes modifications to the modified remaining central parking lot to enhance its accessibility to pedestrians.

The Planning Board was concerned about the possibility of portions of the existing central parking lot being taken over for long term parking or storage of large vehicles or equipment such trailers, boats, construction equipment, motorhomes, buses, storage cubes, shipping containers, or any other objects. No such use of the parking lot area has been requested as part of this application, nor is any such use being authorized. In order to prevent such mis-use, the applicant has offered a

owner's agreement to the town(s), in order to allow the police department(s) to access the site and enforce parking restrictions and preserve fire lanes, as well as prohibit the storage or parking of vehicles, objects or equipment as described.

The Lead Agency also notes that a gate restricting un-authorized access will be re-established as part of this plan. The plans show a six-foot high swinging link gated fence being re-established on the New Windsor portion of the driveway; this requires approval from the New Windsor Planning Board. However, for purposes of this Negative Declaration, the Lead Agency notes that the location of the gate is far enough removed from the highway so that it will not impede fire truck or other truck access to the site, nor will it block the highway. Emergency services personnel will be provided with access. Preventing unrestricted access to the site will reduce the need for enforcement.

Pursuant to Section 239 of General Municipal Law, the plans were referred to the Dutchess County Planning Department, which recommended approval on February 7, 2007.

Based on the offer of the developer's agreement, which must be approved by the attorney(s), and based on the gated access, this action would create no significant planning-related concerns.

### **Archaeological Resources**

When the Town of Cornwall Planning Board circulated its notice of intent to seek Agency status, NYS OPRHP reported that there was an archeological site adjacent to the project area, and recommended completing a Phase I archeological survey for areas of new disturbance. In March, 2007, BTK Associates completed a Phase IA Cultural Resource study for the site. The report indicated that no previous archeological testing had been done on the site, and that previous road fill operations and other site disturbance associated with the existing site's construction in the 1960's have eliminated any archeological potential in most of the property. The report stated that the only portions of the site that were potentially sensitive for prehistoric resources were the south and east portions of the property, and of these, the only possible impact had been in the vicinity of the existing Stormwater pond 2. However, the plans were subsequently revised to completely avoid any disturbance of these potentially sensitive areas, so that the action that are the subject of this action clearly avoid disturbance of any potential archeological resources.

The Phase I-A report stated that a historical archeological site has been reported adjacent to a historic building on Route 32, and another prehistoric site has been reported near the Moodna Creek, but both of these sites are far to the east of the project site, and neither would be affected in any way by the action. There is no historic structure in the vicinity of the site. This action had been designated as a "Type I" due to the proximity of the historic 1755 Edmonston House in the Town of Windsor. However, the new site disturbances that are proposed as part of

this project are primarily in the Town of Cornwall, far separated from the Edmonston House. And although the sight distance improvements that are proposed at the site access in the Town of New Windsor are near to the Edmonston House, the Lead Agency notes that the effect of these improvements is to re-establish the full functionality of the existing site entry features. The minor sight distance clearing and other improvements are nearer to the project site, on the same side of the road as the project site, and will have no effect on the Edmonston House. The warning beacon already exists and would merely be reactivated. In summary, nothing in this land use application would change the appearance or character of the historically significant Edmonston House or its current industrial-commercial environment.

No further action is necessary in this regard.

### **Other Impacts**

Solid waste generation from the proposed re-use has been provided for on the site plan; dumpsters are located around the site and will be served by private carting service. The gated site access will prevent these dumpsters from being mis-used.

Energy consumption from the site is not anticipated not pose any unique or excessive impacts in terms of either volume or type, for re-use of the existing structures. In particular, warehousing and storage components of the site re-use will use negligible amounts of energy. .

No school children will be generated by the proposed use, so no harmful school impacts would be created.

The Lead Agency considered possible health and safety impacts from the proposed site re-use. However, the Lead Agency finds that such concerns are primarily under the jurisdiction of other agencies. For example, with respect to internal building fire safety, this matter is be under the jurisdiction of the New Windsor Fire Inspector and the respective municipal building inspectors. Both municipal Planning Boards' site plan approval is still subject to any necessary requirements of these officials. So too, to the extent that the Remedial Field Investigation under the jurisdiction of the NYSDEC involves any interior building requirements, these would be under the jurisdiction of the municipal building inspectors in coordination with NYSDEC. For purposes of this Negative Declaration, therefore, the Lead Agency finds that the site plan adequately provides for emergency services access and circulation, and further that there is no site work associated with any identified Area of Concern that will affect the ability to implement the site plan. Therefore, there will be no harmful health and safety impacts resulting from approving this site re-use, subject to all other agency approvals and requirements.

No other potentially significant harmful environmental impacts are identified.

**Date Adopted: May 7, 2007**

**Agency Address: Town of Cornwall Planning Board**

**Town Hall – 183 Main Street**

**Cornwall, New York 12518**

**Tel.(845) 534-9429**

**Contact Person: Neil Novesky, Planning Board Chairman**

**Involved and Interested Agencies to receive this mailing**

✓ **Town of New Windsor Planning Board**

**555 Union Avenue**

**New Windsor, New York 12553**

**NYS OPRHP**

**Field Services Bureau – Peebles Island**

**PO Box 189**

**Waterford, NY 12188-0189**

**NYS Department of Transportation – Region 8**

**SEQR Unit (via electronic transmission: [rdillman@dot.state.ny.us](mailto:rdillman@dot.state.ny.us))**

**4 Burnett Boulevard**

**Poughkeepsie, NY 12603**

**NYS DEC Region 3**

**21 South Putt Corners Road**

**New Paltz, NY 12561**

**Orange County Department of Planning**

**124 Main Street**

**Goshen, NY 10924**

**RESOLUTION GRANTING SITE PLAN APPROVAL  
FOR A BUSINESS PARK**

*Vails Gate Business Park  
PB #06-32*

**WHEREAS**, an application was made to the Planning Board of the Town of New Windsor for approval of a site plan by Vails Gate Business Center, LLC (the "applicant") for a project described as the "Vails Gate Business Park";

**WHEREAS**, the subject site consists of 35.9 acres of land and comprised of one tax map parcel in the Town of New Windsor identified on the tax map as section 69, block 4, and lot 3 (SBL 69-4-3) and one tax map parcel in the Town of Cornwall identified on the tax map as section 4, block 3 and lot 1.1 (SBL 4-3-1.1); and

**WHEREAS**, the site is currently developed with three existing buildings: (1) Building 1, a 224,178 square-foot manufacturing facility with eleven loading docks, lying wholly in the Town of Cornwall; (2) Building 2, a 14,328 square foot building that is divided by the municipal boundary, and (3) Building 3, an 18,388 square foot building lying wholly in Cornwall.

**WHEREAS**, the action involves a request for a site plan approval for a mixed re-use of the existing buildings on the site. Specifically, the applicant seeks approval to re-use the total 246,210 square feet in the buildings on the site for a mix of 37.4% light manufacturing (for a total of 92,176 square feet light manufacturing), 61.1% warehousing (for a total of 150,390 square feet of warehousing) and 1.5% mini-storage (for a total of 3,644 square feet of mini-storage). The applicant seeks to internally divide the existing structures for the use of multiple tenants for such uses. The action requires the re-development of the site as a coordinated entity, because the site has functioned and must continue to function as a single entity due to the existing access and other site limitations; and

**WHEREAS**, in the Town of New Windsor, the applicant seeks site plan approval authorizing the redevelopment of a portion of Building 2 (10,564 square feet) contained in the Town of New Windsor to be utilized as self storage; and

**WHEREAS**, the applicant has submitted a fully executed long form Environmental Assessment Form ("EAF") pursuant to the New York State Environmental Quality Review Act ("SEQRA"); and

**WHEREAS**, the Town of Cornwall Planning Board conducted a coordinated SEQRA review for this project which requires, among other things, site plan approval from the Town of Cornwall Planning Board for that portion of the site located in Cornwall; and

**WHEREAS**, during the course of the Planning Board's review of the Applicant's proposed site plan layout, the Planning Board received and considered comments from the public as well as the Town's consultants; and

**WHEREAS**, the Town of New Windsor Planning Board waived the public hearing; and

**WHEREAS**, the Town of Cornwall Planning Board conducted a public hearing, notice of which was sent to property owners in the Town of New Windsor; and

**WHEREAS**, the application and related materials were submitted to the Orange County Planning Department ("OCDP") for its review pursuant to the requirements of the General Municipal Law § 239-m, and OCDP responded on February 9, 2007 recommending approval; and

**WHEREAS**, the Planning Board has carefully considered all of the comments raised by the public, the Board's consultants, and other interested agencies, organizations and officials, including those presented at numerous meetings of the Board as well as those submitted separately in writing; and

**WHEREAS**, the applicant has submitted a proposed site plan consisting of 10 sheets, prepared by T.M. Depuy Engineering and Land Surveying, P.C. dated September 25, 2006 and last revised on April 12, 2007; and

**NOW, THEREFORE**, the Planning Board finds that the applicant has satisfied the requirements of Town Code § 300-86 and approves the site plan subject to the following terms and conditions:

1. The Planning Board has reviewed and relies on the SEQRA negative declaration adopted by the Town of Cornwall Planning Board;

2. The applicant shall pay all outstanding fees due the Town for review of this application;

3. The applicant shall make any required revisions to the site plan to the satisfaction of the Planning Board Engineer and Planning Board Attorney;

4. The applicant shall secure all necessary permits, approvals and authorizations required from any other agency, if required;

5. The applicant shall post security for all off-site and on-site improvements in form and amount acceptable to the Town; and

6. The applicant shall submit proof of satisfaction of the foregoing conditions and submit a site plan for signature within 360 days of the date of this resolution. The Planning Board hereby grants the two (2) ninety (90) day extensions as authorized by Town Code § 300-86(E)(1). This approval will expire on May 3, 2008. No further extensions can be granted.

Please  
Take  
Note →

Upon motion made by Member VanLeeuwen, seconded by Member Schlesinger, the foregoing resolution was adopted as follows:

Member, Daniel Gallagher	<input checked="" type="radio"/> Aye	<input type="radio"/> Nay	<input type="radio"/> Abstain	<input type="radio"/> Absent
Member, Howard Brown	<input checked="" type="radio"/> Aye	<input type="radio"/> Nay	<input type="radio"/> Abstain	<input type="radio"/> Absent
Member, Neil Schlesinger	<input checked="" type="radio"/> Aye	<input type="radio"/> Nay	<input type="radio"/> Abstain	<input type="radio"/> Absent
Member, Henry Vanleeuwen	<input checked="" type="radio"/> Aye	<input type="radio"/> Nay	<input type="radio"/> Abstain	<input type="radio"/> Absent
Chairman, Genaro Argenio	<input checked="" type="radio"/> Aye	<input type="radio"/> Nay	<input type="radio"/> Abstain	<input type="radio"/> Absent

Alternate, Henry Schieble	<input type="radio"/> Aye	<input type="radio"/> Nay	<input type="radio"/> Abstain	<input type="radio"/> Absent
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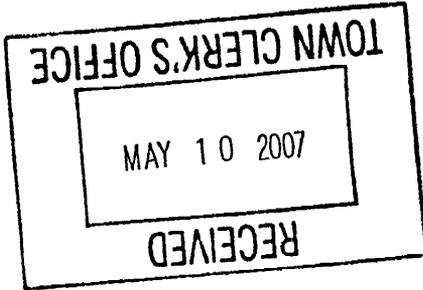
Dated: May 9, 2007  
New Windsor, New York

  
Genaro Argenio, Chairman

Filed in the Office of the Town Clerk on this 10<sup>th</sup> day  
of May, 2007.

*Deborah Green*

Deborah Green  
Town Clerk



PLANNING BOARD  
TOWN OF NEW WINDSOR

AS OF: 05/09/2007

PAGE: 1

LISTING OF PLANNING BOARD ACTIONS

STAGE:

STATUS [Open, Withd]  
O [Disap, Appr]

FOR PROJECT NUMBER: 6-32

NAME: VAILS GATE BUSINESS PARK PA2006-988

APPLICANT: VAILS GATE BUSINESS CENTER LLC

--DATE--	MEETING-PURPOSE-----	ACTION-TAKEN-----
12/13/2006	P.B. APPEARANCE . CORNWALL TO BE L.A. - . AND CORNWALL	RETURN SIMULTANEOUS SUBMISSIONS BETWEEN NW
10/04/2006	WORK SHOP	SUBMIT

PLANNING BOARD  
TOWN OF NEW WINDSOR

AS OF: 05/09/2007

PAGE: 1

LISTING OF PLANNING BOARD AGENCY APPROVALS

FOR PROJECT NUMBER: 6-32

NAME: VAILS GATE BUSINESS PARK PA2006-988

APPLICANT: VAILS GATE BUSINESS CENTER LLC

	DATE-SENT	AGENCY-----	DATE-RECD	RESPONSE-----
REV1	02/26/2007	MUNICIPAL HIGHWAY	/ /	
REV1	02/26/2007	MUNICIPAL WATER	/ /	
REV1	02/26/2007	MUNICIPAL SEWER	/ /	
REV1	02/26/2007	MUNICIPAL FIRE	/ /	
REV1	02/26/2007	NYS DOT	/ /	
ORIG	11/30/2006	MUNICIPAL HIGHWAY	12/11/2006	UNDER REVIEW
ORIG	11/30/2006	MUNICIPAL WATER	02/26/2007	SUPERSEDED BY REV1
ORIG	11/30/2006	MUNICIPAL SEWER	02/26/2007	SUPERSEDED BY REV1
ORIG	11/30/2006	MUNICIPAL FIRE	12/07/2006	DISAPPROVED
		. NEED A 30 FT. FIRE LANE FOR BUILDING 2		NOTE: THIS OFFICE
		. APPROVES SITE PLAN SECTION IN THE TOWN OF NEW WINDSOR ONLY		
ORIG	11/30/2006	NYS DOT	02/26/2007	SUPERSEDED BY REV1

PLANNING BOARD  
TOWN OF NEW WINDSOR

AS OF: 05/09/2007

PAGE: 1

LISTING OF PLANNING BOARD SEQRA ACTIONS

FOR PROJECT NUMBER: 6-32

NAME: VAILS GATE BUSINESS PARK PA2006-988

APPLICANT: VAILS GATE BUSINESS CENTER LLC

	DATE-SENT	ACTION-----	DATE-RECD	RESPONSE-----
ORIG	11/30/2006	EAF SUBMITTED	11/30/2006	WITH APPLICATION
ORIG	11/30/2006	CIRCULATE TO INVOLVED AGENCIES	/ /	
ORIG	11/30/2006	LEAD AGENCY DECLARED	/ /	
ORIG	11/30/2006	DECLARATION (POS/NEG)	/ /	
ORIG	11/30/2006	SCHEDULE PUBLIC HEARING	/ /	
ORIG	11/30/2006	PUBLIC HEARING HELD	/ /	
ORIG	11/30/2006	WAIVE PUBLIC HEARING	/ /	
ORIG	11/30/2006	FINAL PUBLIC HEARING	/ /	
ORIG	11/30/2006	PRELIMINARY APPROVAL	/ /	
ORIG	11/30/2006	LEAD AGENCY LETTER SENT	/ /	





**McGOEY, HAUSER and EDSALL**  
**CONSULTING ENGINEERS P.C.**

RICHARD D. MCGOEY, P.E. (NY & PA)  
 WILLIAM J. HAUSER, P.E. (NY & NJ)  
 MARK J. EDSALL, P.E. (NY, NJ & PA)  
 JAMES M. FARR, P.E. (NY & PA)

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 MJE@MHEPC.COM

**TOWN OF NEW WINDSOR**  
**PLANNING BOARD**  
**DISCUSSION COMMENTS**

The project listed below is on the Planning Board agenda for an informal discussion. The information provided below is provided in a general manner and is not the result of a complete review of plans or the discussion matter; as such, the comments below should be considered general, informal and potentially incomplete at this time. Further review will be made only if so directed by the Planning Board.

**SUBJECT:** VAILS GATE  
**REASON FOR DISCUSSION:** UPDATE THE BOARD ON PROGRESS WITH TOWN OF CORNWALL P.B.

1. The applicant has a current application before this Planning Board, as well as the Town of Cornwall Planning Board (since the property is split with the Town Line). Cornwall's Planning Board is the Lead Agency under SEQRA.

The applicant has appeared at the Town of Cornwall Planning Board at several meetings, and currently has complied with all comments and requests of that Board and the Cornwall consultants. The Cornwall Planning Board authorized the Attorney to prepare the necessary resolutions for a "neg dec" and conditional site plan approval. They are scheduled for the Cornwall meeting of 7 May 2007 for approval action.

2. The applicant has requested this appearance with the New Windsor Planning Board to update this Board on the progress and plan modifications made with the Cornwall Board, and as importantly, review the traffic aspect for this site's access to NYS Route 94 (just east of the NYS Thruway overpass).
3. The Board should discuss this matter with the applicant, and if acceptable, authorize the Attorney to prepare the necessary resolutions for consideration at a May Planning Board meeting.

**REGIONAL OFFICES**

• 111 WHEATFIELD DRIVE – SUITE ONE • MILFORD, PENNSYLVANIA 18337 • 570-296-2765 •  
 • 540 BROADWAY • MONTICELLO, NEW YORK 12701 • 845-794-3399 •



# Town of New Windsor

555 Union Avenue  
New Windsor, New York 12553  
Telephone: (845) 563-4615  
Fax: (845) 563-4693

## OFFICE OF THE PLANNING BOARD

1 March 2007

Mr. Richard Dillmann, P.E., Regional Traffic Engineer  
NYS Department of Transportation, Region 8  
4 Burnett Boulevard  
Poughkeepsie, N.Y. 12603

SUBJECT: VAILS GATE BUSINESS PARK SITE PLAN (FORMER TARKETT FACILITY)  
NEW WINDSOR PLANNING BOARD NO. 06-32

Dear Mr. Dillmann:

The Town of New Windsor Planning Board has received an application for site plan approval of an amendment to the use of the former Tarkett site located off NYS Route 94 (just west of Vails Gate) within the Town. The project spans the Town Line, and also involves a concurrent application to the Town of Cornwall Planning Board (their App. No. 06-17). The Planning Board has determined that the applicant will be required to obtain a Highway Work Permit from your Department since certain access and safety improvements are depicted on the plans.

We are forwarding herewith a copy of the plans submitted with the application for your review and comment. We request that you notify the Planning Board of any concerns regarding this application, which should be considered by the Board during their review of the project. We will share all comments and coordinate with the Town of Cornwall Planning Board.

*It is not the intent that these plans be considered the plans required for the Permit application, as these will be the responsibility of the applicant following site plan approval from the Town.*

We look forward to your input regarding this application before the Board.

Very truly yours,

TOWN OF NEW WINDSOR PLANNING BOARD

Mark J. Edsall, P.E., P.P.  
Engineer for the Planning Board

MJE/st  
NW06-32-NYS DOT-Ref-03-01-07.doc

## **LEGAL NOTICE**

Notice is hereby given that the Planning Board of the Town of Cornwall, County of Orange, State of New York, will hold a public hearing at the Town of Cornwall Town Hall, 183 Main Street, Cornwall, New York, on February 5, 2007, at 7:30 p.m. or as soon thereafter as the matter can be heard, on the approval of a proposed site plan of the lands of Vails Gate Business Park, LLC, located at 1215 NYS Route 94. The property is identified as Town of Cornwall Tax Map No. 4-3-1.1 and Town of New Windsor Tax Map No. 69-4-3.

The map of the proposed site plan is on file and may be inspected at the Town of Cornwall, Town Clerk's Office, Town Hall, 183 Main Street, Cornwall, New York 12518, and Town of New Windsor, Planning Board Office, Town Hall, 555 Union Avenue, New Windsor, New York 12553, prior to the public hearing.

Dated: January 9, 2007

By Order of the Town of Cornwall Planning Board  
Neil Novesky, Chairman

VAILS\_GATE\_BUSINESS\_PARK\_(06-32)

MR. ARGENIO: Last thing on the agenda is Vails Gate Business Park tonight and this is the former Tarkett property folks over on 94 near the Thruway. This application proposes redevelopment of the former Tarkett facility 36 acres of land, general manufacturing industrial processing and self-storage. The plan was reviewed on a concept basis only. How you doing folks? Can I have your name please for the record?

MR. WOLINSKY: My name is Larry Wolinsky, I'm with the firm of Jacobowitz & Gubits. This is Jeff Kessler, he's the property owner.

MR. ARGENIO: Tell us what you folks want to do.

MR. WOLINSKY: I'm going to try and cover as best I can. So as you mentioned, this is known as the Vails Gate Business Park. The applicant is seeking a site plan amendment for the redevelopment of the Tarkett industrial building, site is divided by municipal line, most of the property here is in Cornwall, okay, and the front portion here is in New Windsor. I'm sure many of you are familiar with it. The applicant has already appeared before the Town of Cornwall Planning Board several times, the planning board in Cornwall has declared its intent to be lead agency for the environmental review, a circulated notice to that effect and you should see that as part of your records.

MR. ARGENIO: And we do have that.

MR. WOLINSKY: The first building on the property as we're moving in from the state highway is this building right here, most of that building is in New Windsor and they're proposing to divide that into some self-storage units, mini-warehouses, and mini-warehouse, this is, that portion is not design shopping district and

mini-warehouses are permitted there, the second building which is in Cornwall is the large building that you're familiar with, we're proposing to divide that into a number of tenantable spaces but to be used for the exact same use that it was previously used for.

MR. ARGENIO: You're going to make tile in there?

MR. WOLINSKY: No.

MR. KESSLER: It was manufacturing and warehouse.

MR. WOLINSKY: When I say the same use, I'm talking generic fashion for the zone.

MR. SCHLESINGER: But what you're going to do is have 15 tenants that may be making other products or whatever it may be?

MR. WOLINSKY: Correct.

MR. KESSLER: Or might be doing warehousing.

MR. WOLINSKY: Correct, there's been a traffic study that's been prepared for this by Phil Greely, John Collins Associates, that's part of the SEQRA documentation and that's currently under review and in Mark's comments which I do have a copy.

MR. ARGENIO: Let me interrupt you for one second but I'm going to tell you this right now so you can hear it, we're going to vote on the SEQRA thing, the lead agency thing and as far as I'm concerned, Cornwall can take it, I want the traffic information here.

MR. WOLINSKY: Yeah.

MR. ARGENIO: I want to see it, I want a presentation by the--

MR. WOLINSKY: Okay.

MR. ARGENIO: When the time comes.

MR. WOLINSKY: You want Phil to come here and give you the presentation?

MR. ARGENIO: This is a smoking hot issue in our town and it's important to the residents of this town, it's important to me and it's important to these people who sit here.

MR. WOLINSKY: I agree with you a hundred percent, no problem.

MR. ARGENIO: Go ahead.

MR. WOLINSKY: Pretty much that's it, I mean, we're working with, as we're working with both towns we're working with the fire departments in both towns to make sure that area's covered and essentially we're here tonight to get your comments, one of which you just gave us and then also from a procedural standpoint to just have the board acknowledge that Cornwall will be handling the lead agency aspects for the SEQRA review and then we'll continue to work in the workshop setting in cleaning up Mark's comments from his memo and getting the plans in shape before we come back to you.

MR. ARGENIO: I'll accept a motion that the Town of New Windsor Planning Board accept Cornwall being lead agency under the SEQRA process.

MR. GALLAGHER: So moved.

MR. MINUTA: Second it.

MR. ARGENIO: Motion has been made and seconded that the Town of New Windsor Planning Board accept formally that Cornwall will be the lead agency under the SEQRA

process. No further discussion, roll call.

ROLL CALL

MR. GALLAGHER	AYE
MR. BROWN	AYE
MR. MINUTA	AYE
MR. SCHLESINGER	AYE
MR. ARGENIO	AYE

MR. WOLINSKY: Essentially, we're done. Any other comments that you have that you want to give us that we can take into consideration, again, we have Mark's letter, these are very much as he pointed out the same comments that we received in the Town of Cornwall, we have to go through a process to clean it up.

MR. ARGENIO: That traffic is important to us, I don't want to beat you to death with it, but I'm going to tell you I want Phil here with his dog and pony show, I want to see it, I want to see the level of services, trips generated before, trips generated subsequent to and as I said not cause I want to be difficult but it's a real smokin' hot issue in this town. When the Hannafords went up, I sat on this board, Neil, were you here?

MR. SCHLESINGER: Yes.

MR. ARGENIO: Neil and I can tell you this room was full and there was only one issue they were concerned about.

MR. WOLINSKY: You may not remember me.

MR. ARGENIO: I do remember you, one issue and one issue only and your engineer was Creighton Manning?

MR. WOLINSKY: That's correct.

MR. ARGENIO: You know what, they knew what they were doing more than the state, they took what the state couldn't get right and they made it right. If you remember my comment at the time I looked at your traffic engineer, tall guy, I don't remember his name, and I said this better not be smoking mirrors cause you're going to be back here again and he got it right.

MR. WOLINSKY: That was a huge issue on that project.

MR. ARGENIO: I don't want to kill you with it but I want to hear from Phil.

MR. CORDISCO: Two minor points and I think as Mark pointed out they should make simultaneous submissions to both Cornwall and New Windsor in the sense that there may be appearances necessary in Cornwall because perhaps most of the issues are being reviewed by Cornwall but I think both towns should have the same set of plans.

MR. ARGENIO: Wait a second, let me just, when you say simultaneous, maybe I'm not understanding what you're saying, Dominic, simultaneous submissions, what exactly do you mean by that?

MR. CORDISCO: If they're submitting revised plans to Cornwall, I think revised plans should also come here but they don't necessarily need to be on our agenda.

MR. ARGENIO: I don't want to see every time you go to Cornwall I don't want to see you, I don't want to see you.

MR. WOLINSKY: We don't want to see you either, I understand.

MR. ARGENIO: What I want to see is I want to see when they're to a point with Cornwall where this thing is almost at fruition or it's at fruition I want to see

you then and I want to see Phil here and Joe and Howard and you and me and Neil are going to have to talk about the traffic to make sure we understand the impact on your town and that's how we'll do it but I don't want to see you 12 times, work it out with them.

MR. WOLINSKY: We'll copy you.

MR. ARGENIO: How does that work with a public hearing on this?

MR. CORDISCO: Well, you could have a joint public hearing but this is--

MR. ARGENIO: You know what I want to do, that's exactly what I want to do, I want Cornwall to have a public hearing because they're going to have it and it's going to be required and I want you folks to do a mailing to the people in New Windsor, have it down there.

MR. CORDISCO: The mailing that would be the most prudent way to do it.

MR. ARGENIO: I think Cornwall will have the public hearing and include the New Windsor folks within the 500 feet or however it works in the mailing, let them go to Cornwall because the bulk of this is in Cornwall. The only issue that we really have here in my estimation is the traffic issue. Do you have a problem with that?

MR. WOLINSKY: No, with one caveat and again, I'm not the person handling it in Cornwall but there has been some discussion about the possibility of public hearing may not be necessary under the regulations there but--

MR. CORDISCO: I actually think it's the reverse, New Windsor has the ability to waive the public hearing on site plan, Cornwall does not.

MR. WOLINSKY: Well, okay, wherever we have it we'll notice New Windsor.

MR. DENECA: The only comment--

MR. ARGENIO: Include Neil in the mailing.

MR. DENECA: You may just want to consider whether or not any of the board here may want to be present at the public hearing, I'm not sure if Dominic--

MR. SCHLESINGER: I'm going to get a letter.

MR. ARGENIO: You guys want letters too?

MR. BABCOCK: Why don't they do one to the Town of New Windsor Planning Board, Myra will pass it out.

MR. ARGENIO: Let me give you some feedback here. My confidant, two of my confidants will be at that public hearing, one is Mark Edsall and the other one is Dominic Cordisco. I don't intend to, I can get feedback but sir do notify Myra who will notify these members.

MR. WOLINSKY: Because you're an adjoining municipality we're legally required.

MR. SCHLESINGER: I want to make sure that the required residents get notification of the public hearing.

MR. ARGENIO: I just told him that.

MR. WOLINSKY: We just said we'll acknowledge that, we'll notify the New Windsor residents as part of the Cornwall hearing.

MR. SCHLESINGER: I have another question. What's the facility all the way to your left, lean-tos?

MR. WOLINSKY: It's an existing building that will be divided, same use divided into five tenanted spaces.

MR. SCHLESINGER: I don't know what you mean by lean-to.

MR. BABCOCK: That's a shed roof type thing on the front.

MR. SCHLESINGER: Is that going to be enclosed?

MR. WOLINSKY: It's an enclosed building.

MR. KESSLER: It's a pole barn.

MR. SCHLESINGER: The type of building is called a lean-to but all the, each cubicle will be a self-contained?

MR. KESSLER: Right.

MR. MINUTA: That's currently a habitable building?

MR. KESSLER: Yes.

MR. SCHLESINGER: You need to address the traffic flow there as well.

MR. WOLINSKY: Yes, absolutely internal circulation.

MR. SCHLESINGER: Fire department.

MR. WOLINSKY: Yes and we're meeting with both New Windsor and Cornwall Fire Department.

MR. ARGENIO: You folks have, I'm not going to go through the items but there's a lot of items they're on the plans that need to be cleaned up and the reason I'm not going to get too twisted up about them is cause

Cornwall is going to review this fairly closely.

MR. WOLINSKY: You know, Mr. Chairman, when you're dealing, you know, it's an engineering challenge when you're dealing with an existing facility and you're doing their rehab so we'll get it right.

MR. KESSLER: We're having a workshop session tomorrow with them.

MR. ARGENIO: It's going to be a construction challenge to whoever makes the improvements, I understand, I agree.

MR. MINUTA: Real quick, big picture this site existed as manufacturing for Tarkett, are we changing that?

MR. KESSLER: Manufacturing and warehouse is, in other words, they made tiles and they warehoused tiles.

MR. MINUTA: And you're doing what, the same?

MR. KESSLER: We're going to bring in tenants to do the same thing, either manufacture or warehouse.

MR. MINUTA: Same use and occupancy?

MR. WOLINSKY: Same use and occupancy, yes.

MR. CORDISCO: I have only one minor and he can do the that, I wanted to share with the board, the Cornwall Planning Board had made a request that Mr. Kessler see whether or not CSX which has the railroad spur that goes passed there would actually remove the railroad across Route 94 because it hasn't been used in many years and they made that request, my understanding is that CSX said not in their lifetime.

MR. ARGENIO: You'll see Jesus before you see that. I don't mean to be blunt but you'll see Jesus before you

December 13, 2006

57

see that.

MR. CORDISCO: It was a valid request, it hasn't been used, it's mandatory stuff for school buses, it adds to congestion, it made, seems to make that request but they did follow up but it's not within their power to do it.

MR. ARGENIO: Thank you, Dominic. Anybody else have any comments on this application? We'll see you folks again. Remember traffic.

MR. WOLINSKY: Thank you.

JERRY'S\_STORAGE\_-\_DISCUSSION

---

MR. SCHLESINGER: And the other question I had is with Jerry's Storage building, we were talking about the existing storage building on the corner that Jerry built, that he sold, I guess?

MR. BABCOCK: That's correct.

MR. SCHLESINGER: I believe that Jerry said or somebody said that unfortunately the landscaping died and it's a little bit of a more of an eyesore than it was when Jerry had it. All right, how do we enforce that?

MR. BABCOCK: We can take care of that.

MR. SCHLESINGER: We make an effort to require people to have landscaping for obvious reasons and then it dies and we're wasting everybody's time.

MR. MINUTA: If I may, other towns post a bond on the landscaping for one to two years and what happens is if the landscaping dies within that time, the bond is in place for that landscaping to be replaced. Now typically if you're going to plant landscaping it's going to die within the first year it's going to die so that's the way they handle it, the bond is removed after the two years.

MR. BABCOCK: We're in the process, we're going to do something about that, I think you talked to me, I got to be honest with you, I can't tell you I don't remember whether I sent somebody there or not but my intention was we're going to send somebody there, we're going to tell them to straighten it up, if he doesn't do it the way that we enforce it we give him a violation of a site plan, site plan calls for trees.

MR. SCHLESINGER: More work for your office.

MR. BABCOCK: We'll get them to take care of it.

MR. GALLAGHER: On the same subject for the site plan wasn't that also when they put the two extra buildings they didn't keep the same conformity with the peak.

MR. BABCOCK: Myra, could you make a note for me?

MR. MINUTA: There's no conformity there with what they put up new, what they did is the first shot everything is homogeneous and cohesive.

MR. SCHLESINGER: Who put up the next, the building, Jerry or the new owner?

MR. BABCOCK: New owner.

MR. MINUTA: He was approved for the site.

MR. BABCOCK: We'll send somebody out there.

MR. ARGENIO: Thank you. Motion to adjourn?

MR. SCHLESINGER: So moved.

MR. MINUTA: Second it.

ROLL CALL

MR. GALLAGHER	AYE
MR. BROWN	AYE
MR. MINUTA	AYE
MR. SCHLESINGER	AYE



**McGOEY, HAUSER and EDSALL  
CONSULTING ENGINEERS P.C.**

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**WRITER'S E-MAIL ADDRESS:**  
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**TOWN OF NEW WINDSOR**  
**PLANNING BOARD**  
**REVIEW COMMENTS**

**PROJECT NAME:** VAILS GATE BUSINESS PARK  
(REDEVELOPMENT OF TARKETT SITE)  
**PROJECT LOCATION:** OFF NYS ROUTE 94  
SECTION 69 – BLOCK 4 – LOT 3  
**PROJECT NUMBER:** 06-32  
**DATE:** 13 DECEMBER 2006  
**DESCRIPTION:** THE APPLICATION PROPOSES THE REDEVELOPMENT OF THE FORMER TARKETT FACILITY (APPROX. 36 ACRES TOTAL LAND AREA) WITH GENERAL MANUFACTURING, INDUSTRIAL PROCESSING, AND SELF-STORAGE. THE PLAN WAS REVIEWED ON A CONCEPT BASIS ONLY.

1. The project spans the Town of Cornwall – Town of New Windsor line, and will be the subject of a concurrent review by the Cornwall Planning Board (Cornwall Application 06-17).

The property is located in the C Zoning District of the Town. The use proposed in the New Windsor portion of the site is Mini-Warehouse, which is use A-6 of the zone. The bulk information shown on the plan requires several corrections. The applicant should refer to the current bulk tables and correct the plan.

2. It is my understanding these are the same plans as submitted to the Cornwall Planning Board. *(if they are not, the applicant should insure that the sets are identical on an ongoing basis since both boards will be asked to co-stamp and co-sign one approved set).* The following comments are similar to those provided to the Cornwall Board on December 4<sup>th</sup>:

- **Plan Legibility** – the plans are difficult to follow, doing a poor job in distinguishing what is existing vs. what is proposed as part of the application.

Existing pavement edges vs. proposed edges of pavement should be defined. It would be helpful if the plan clearly call out proposed widths in areas where driveways are to be widened, or shifted. As well, the plan should address what is to be done to areas where pavement is presumably to be removed (ie topsoil and seed ?).

**REGIONAL OFFICES**

• 111 WHEATFIELD DRIVE – SUITE ONE • MILFORD, PENNSYLVANIA 18337 • 570-296-2765 •  
• 540 BROADWAY • MONTICELLO, NEW YORK 12701 • 845-794-3399 •

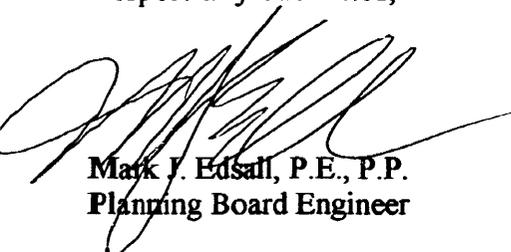
- ***On-Site Parking*** – parking calculation for the overall site must be confirmed vs. the specific uses identified in each Town. With regard to the parking layout as depicted, note the following comments:
  - The handicapped spaces depicted on the plans do not meet State Code requirements relative to access aisles and signage. (detail is also incorrect).
  - Parking space typical detail is duplicated on sheets SP5 and SP6. The dimensions indicated on detail sheet SP6 are incorrect. The details should specify minimum aisle widths.
  - I question if the parking, as it is distributed, and where it is located (relative to the buildings themselves) will be functional.
  - A paving detail is provided on the detail sheet, although it is noted that it is difficult to determine where new paving is to be installed. Also, for areas which are to be widened, is the entire width to receive a top course?
- ***Stormwater*** – Once the issues regarding plan clarity are resolved, we will complete our detailed review of this aspect of the application.
- ***Fire Protection*** – As the Cornwall Board was advised, the New Windsor Fire Inspector's Office and the Cornwall Building Inspector have noted concerns regarding the sprinkler systems of the buildings. This issue and the fire lane identification, will be subject to further input.
- ***Details*** – Notwithstanding the fact that details are provided for fencing, lighting, bollards, guide rails, sidewalks, paving, etc., I am not clear by looking at the plans where such improvements are proposed.

3. We have received a traffic impact study for the project. Our initial review comments are as follows:

- Page 3 – please note that Rt. 94 terminates in the Town of New Windsor (not in the City of Newburgh).
- Page 5 – please indicate under B and in Table No. 1 the ITE classifications and trip generation rates for the associated prior use and proposed uses.
- Page 5 – please quantify the volume of traffic that was associated with the prior Tarkett use during full operation (for comparison purposes to the proposed site plan uses). The existing traffic volumes are obviously not representative of the operational Tarkett site.
- Page 6 – under D please advise if any proposed (but not built) projects in New Windsor and/or Cornwall have been considered. List any such projects.

- General – we understand that the Cornwall Board’s request that you seek removal of the railroad siding/crossing of Rt. 94 has been disapproved by CSX. Please investigate alternatives to improve the access conditions.
  - General – a concept plan should be prepared to depict the possible improvements at the site access. An eastbound deceleration lane at the site entrance should be considered.
  - General – in connection with sight distance improvements indicated, please provide actual current and improved (after clearing) sight distances for the site entrance, and compare the same to the recommended standards of NYSDOT.
  - Site Generated Traffic Volumes – please indicated the relative amounts of passenger car and truck type traffic anticipated for the site, as well as comparative numbers for the prior use.
4. The Cornwall Planning Board has indicated their intent (desire) to assume the Lead Agency position for the project. Consistent with prior applications split by the Town line, since the majority of the use is in Cornwall, ***I recommend this Board formally (by resolution) concur with Cornwall being the Lead Agency under SEQRA.***
  5. Our review will continue as the plans are modified, and we will continue to coordinate with the Cornwall Planning Board.

Respectfully Submitted,



Mark J. Edsall, P.E., P.P.  
Planning Board Engineer

MJE/st  
NW06-32-13Dec06.doc



# Town of New Windsor

555 Union Avenue  
New Windsor, New York 12553  
Telephone: (845) 563-4631  
Fax: (845) 563-3101

## Assessors Office

January 18, 2007

Vails Gate Business Park  
Former Tarkett Building  
Vails Gate, NY 12584

Re: 69-4-3

ZPB #: 06-32 (5)

Dear Vails Gate Business Park:

According to our records, the attached list of property owners are abutting and across any street of the above referenced property, also please be advised your property is also abutting to the town of Cornwall.

The charge for this service is \$25.00 minus your deposit of \$25.00.

There is no balance due.

Sincerely,

J. Todd Wiley, IAO  
Sole Assessor

JTW/rah  
Attachments

CC: Myra Mason, Zoning Board

69-1-2  
GEORGE &  
THERESA HOPKINS  
PO BOX 31  
VAILS GATE, NY 12584

69-1-4.4, 69-4-2.2  
NEW YORK CENTRAL LINES  
C/O CSX  
500 WATER ST. (J-910)  
JACKSONVILLE, FL 32202

69-4-2.1  
CORNWALL COAL & SUPPLY  
PO BOX 520  
CORNWALL, NY 12518

69-4-4.1, 69-4-4.2  
VAILS GATE REALTY INC.  
1067 RT. 94  
NEW WINDSOR, NY 12553

69-4-6.2  
WEST POINT TOURS, INC.  
PO BOX 125  
VAILS GATE, NY 12584

**New York State Department of Environmental Conservation  
Division of Water, Region 3**

100 Hillside Avenue – Suite 1W, White Plains, New York 10603-2860  
Phone: (914) 428-2505 • FAX: (914) 428-0323  
Website: [www.dec.state.ny.us](http://www.dec.state.ny.us)



January 11, 2007

**RECEIVED**

JAN 12 2007

**BUILDING DEPARTMENT**

Thomas DePuy, P.E., L.S.  
T.M. DePuy Engineering and Land Surveying, P.C.  
2656 Route 302  
Middletown, NY 10941

.RE: Vails Gate Business Center Stormwater Pollution Prevention Plan  
Town of Cornwall and Town of New Windsor  
SPDES General Permit for Construction Activity, GP-02-01

Dear Mr. DePuy:

We have received the erosion and sediment control plan for the above referenced project. Please note that a full Stormwater Pollution Prevention Plan is required for this site.

If you have any questions, I can be reached at the above phone number, extension 354.

Sincerely,

*Natalie Browne*  
Natalie Browne  
Environmental Program Specialist

cc: Town of Cornwall Building Department  
Town of New Windsor Building Department

P.B.# 06-32

cc: ME  
DC



**Town of New Windsor**  
555 Union Avenue  
New Windsor, NY 12553  
(845) 563-4611

**RECEIPT**  
**#933-2006**

12/04/2006

Depuy Engineering

Received \$ 125.00 for Planning Board Fees, on 12/04/2006. Thank you for stopping by the Town Clerk's office.

As always, it is our pleasure to serve you.

**Deborah Green**  
Town Clerk

#06-32

PLANNING BOARD  
TOWN OF NEW WINDSOR

AS OF: 12/04/2006

PAGE: 1

LISTING OF PLANNING BOARD FEES  
ESCROW

FOR PROJECT NUMBER: 6-32  
NAME: VAILS GATE BUSINESS PARK PA2006-988  
APPLICANT: VAILS GATE BUSINESS CENTER LLC

--DATE--	DESCRIPTION-----	TRANS	--AMT-CHG	-AMT-PAID	--BAL-DUE
11/30/2006	REC. CK. #17074	PAID		750.00	
		TOTAL:	0.00	750.00	-750.00

*J. Pa.*  
12/4/06

P.B.#06-32  
CC: M.E.  
D.C.



STATE OF NEW YORK  
DEPARTMENT OF TRANSPORTATION  
REGION EIGHT  
4 BURNETT BOULEVARD  
POUGHKEEPSIE, NEW YORK 12603  
www.nysdot.gov

ROBERT A. DENNISON III, P.E.  
REGIONAL DIRECTOR  
March 26, 2007

ASTRID C. GLYNN  
ACTING COMMISSIONER

Mark Edsall  
Planning Board Engineer  
Town of New Windsor  
555 Union Avenue  
New Windsor, NY 12553

Re: SEQR 07-0041  
Vails Gate Business Park  
Route 94

Dear Mr. Edsall:

The SEQR unit has completed its review of the information submitted regarding the subject project. The Department conceptually agrees with the proposed improvements to the existing driveway. A detailed engineering review will be done during the Highway Work Permit review process. The applicant should be directed to contact the Department's local permit inspector to initiate the detailed review process.

Siby Zachariah  
Permit Inspector  
112 Dickson Street  
Newburgh, NY 12550

If you have any questions please feel free to contact me at (845) 575-6040

Very truly yours,

A handwritten signature in black ink, appearing to read "Glenn Boucher".

Glenn Boucher  
Regional SEQRA Coordinator

cc S. Zacariah, Permit Inspector, Res 8-4  
Phil Grealy, John Collins Engineers

PLANNING BOARD  
TOWN OF NEW WINDSOR

AS OF: 12/13/2006

PAGE: 1

LISTING OF PLANNING BOARD AGENCY APPROVALS

FOR PROJECT NUMBER: 6-32

NAME: VAILS GATE BUSINESS PARK PA2006-988

APPLICANT: VAILS GATE BUSINESS CENTER LLC

DATE-SENT	AGENCY-----	DATE-RECD	RESPONSE-----
ORIG 11/30/2006	MUNICIPAL HIGHWAY	12/11/2006	UNDER REVIEW
ORIG 11/30/2006	MUNICIPAL WATER	/ /	
ORIG 11/30/2006	MUNICIPAL SEWER	/ /	
ORIG 11/30/2006	MUNICIPAL FIRE	12/07/2006	DISAPPROVED
	. NEED A 30 FT. FIRE LANE FOR BUILDING 2		NOTE: THIS OFFICE
	. APPROVES SITE PLAN SECTION IN THE TOWN OF NEW WINDSOR ONLY		
ORIG 11/30/2006	NYS DOT	/ /	

PLANNING BOARD  
TOWN OF NEW WINDSOR

AS OF: 12/13/2006

PAGE: 1

LISTING OF PLANNING BOARD ACTIONS

STAGE:

STATUS [Open, Withd]  
O [Disap, Appr]

FOR PROJECT NUMBER: 6-32

NAME: VAILS GATE BUSINESS PARK PA2006-988

APPLICANT: VAILS GATE BUSINESS CENTER LLC

--DATE--	MEETING-PURPOSE-----	ACTION-TAKEN-----
10/04/2006	WORK SHOP	SUBMIT

**FIRE INSPECTOR'S  
INTER-OFFICE CORRESPONDENCE**

**TO: Genaro Argenio, Planning Board Chairman**

**FROM: Kenneth Schermerhorn, Asst. Fire Inspector**

**SUBJECT: PB-06-32**  
**Vails Gate Business Park**  
**SBL: 69-4-3**

**DATE: December 7, 2006**

**Fire Prevention Reference Number: FPS-06-054**

**A review of the above referenced site plan has been conducted and is unacceptable for the following reason:**

- 1) Need a 30 ft. Fire Lane for building # 2**

**\* Note: This office approves site plan section in the Town of New Windsor ONLY !**



# Town of New Windsor

555 Union Avenue  
New Windsor, New York 12553  
Telephone: (845) 563-4615  
Fax: (845) 563-4693

## OFFICE OF THE PLANNING BOARD

### PROJECT REVIEW SHEET

**RECEIVED**  
DEC 04 2006  
N.W. HIGHWAY DEPT.

**TO: HIGHWAY DEPARTMENT**

P.B. FILE #06-32      DATE RECEIVED: 11-30-06      TAX MAP #69-

**PLEASE RETURN COMPLETED FORM TO MYRA  
BY: 12-11-06 TO BE ON AGENDA FOR THE 11-13-06 PLANNING BOARD  
MEETING.**

THE MAPS AND/OR PLANS FOR:

### VAILS GATE BUSINESS PARK

Applicant or Project Name

SITE PLAN - XXX, SUBDIVISION \_\_\_\_\_, LOT LINE CHANGE \_\_\_\_\_,  
SPECIAL PERMIT \_\_\_\_\_

HAVE BEEN REVIEWED BY THE UNDERSIGNED AND ARE:

**APPROVED:**

Notes: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**DISAPPROVED:**

Notes: Under advisement.  
\_\_\_\_\_  
\_\_\_\_\_

Signature: Anthony [Signature]      12-11-06  
Reviewed by      date



**McGOEY, HAUSER and EDSALL  
CONSULTING ENGINEERS P.C.**

RICHARD D. McGOEY, P.E. (NY & PA)  
WILLIAM J. HAUSER, P.E. (NY & NJ)  
MARK J. EDSALL, P.E. (NY, NJ & PA)  
JAMES M. FARR, P.E. (NY & PA)

Main Office  
33 Airport Center Drive  
Suite #202  
New Windsor, New York 12553  
(845) 567-3100  
e-mail: mhery@mhepc.com

Regional Office  
507 Broad Street  
Milford, Pennsylvania 18337  
(570) 296-2765  
e-mail: mhpea@mhepc.com

Writer's E-mail Address:  
mje@mhepc.com

**PLANNING BOARD WORK SESSION  
RECORD OF APPEARANCE**

1003

TOWN / VILLAGE OF: New Windsor P/B APP. NO.: \_\_\_\_\_

WORK SESSION DATE: 4 Oct 06 PROJECT: NEW  OLD \_\_\_\_\_

REAPPEARANCE AT W/S REQUESTED: later RESUB. REQ'D: full off

PROJECT NAME: Kessler

REPRESENTATIVES PRESENT: Tom Ryan etc

MUNICIPAL REPS PRESENT: BLDG INSP. \_\_\_\_\_ FIRE INSP.  Ken  
ENGINEER  PLANNER \_\_\_\_\_  
P/B CHMN \_\_\_\_\_ OTHER \_\_\_\_\_

ITEMS DISCUSSED:  
primarily discussion about  
fire access  
30' drive lanes  
FDC location - Fire Dept Connection  
sprinkler 6-zones  
- they can coord FD  
issues directly w/ Ken S.  
will be preparing app

STND CHECKLIST: PROJECT TYPE  
DRAINAGE \_\_\_\_\_ SITE PLAN  
DUMPSTER \_\_\_\_\_ SPEC PERMIT  
SCREENING \_\_\_\_\_ L L CHG.  
LIGHTING \_\_\_\_\_ SUBDIVISION  
(Streetlights) \_\_\_\_\_ OTHER  
LANDSCAPING \_\_\_\_\_  
BLACKTOP \_\_\_\_\_  
ROADWAYS \_\_\_\_\_  
APPROVAL BOX \_\_\_\_\_

PROJECT STATUS:  
ZBA Referral: \_\_\_\_\_ Y \_\_\_\_\_ N  
Ready For Meeting \_\_\_\_\_ Y \_\_\_\_\_ N  
Recommended Mtg Date \_\_\_\_\_

# TOWN OF NEW WINDSOR

555 UNION AVENUE  
NEW WINDSOR, NEW YORK 12553  
Telephone: (845) 563-4615  
Fax: (845) 563-4689

## PLANNING BOARD APPLICATION

### TYPE OF APPLICATION (check appropriate item):

Subdivision \_\_\_\_\_ Lot Line Change \_\_\_\_\_ Site Plan  Special Permit \_\_\_\_\_

Tax Map Designation: Sec. 69 Block 4 Lot 3

### BUILDING DEPARTMENT PERMIT NUMBER:

PA 2006 - 988  
MUST FILL IN THIS NUMBER

1. Name of Project Vails Gate Business Park

2. Owner of Record Vails Gate Business Center LLC Phone 973-340-0202

Address: 460 Getty Avenue Clifton New Jersey 07011  
(Street Name & Number) (Post Office) (State) (Zip)

3. Name of Applicant Vails Gate Business Center LLC Phone 973-340-0202

Address: 460 Getty Avenue Clifton New Jersey 07011  
(Street Name & Number) (Post Office) (State) (Zip)

T.M. DePuy Engineering

4. Person Preparing Plan & Land Surveying, P.C. Phone 845-361-5421

Address: 2656 Route 302 Middletown New York 10941  
(Street Name & Number) (Post Office) (State) (Zip)

5. Attorney Jacobowitz & Gubits, P.C. Phone 845-778-2121

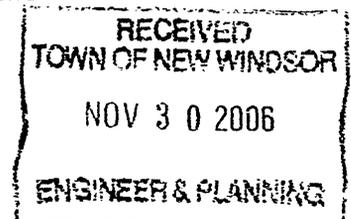
Address 158 Orange Avenue, PO Box 367 Walden, New York 12586  
(Street Name & Number) (Post Office) (State) (Zip)

6. Person to be notified to appear at Planning Board meeting:

Thomas M. DePuy  
T.M. DePuy Engineering 845-361-5421 845-361-5229  
& Land Surveying, P.C. (Name) (Phone) (fax)

7. Project Location: On the South side of NYS Route 94  
(Direction) (Street)

8. Project Data: Acreage 6.8 Zone C School Dist. Newburgh



9. Is this property within an Agricultural District containing a farm operation or within 500 feet of a farm operation located in an Agricultural District? Yes \_\_\_\_\_ No X

\*This information can be verified in the Assessor's Office.

\*If you answer yes to question 9, please complete the attached Agricultural Data Statement.

10. Detailed description of Project: (Use, Size, Number of Lots, etc.) Redevelopment of a portion of Building 2 (10,564 SF) contained in the Town of New Windsor proposed for utilization as self storage.

11. Has the Zoning Board of Appeals Granted any Variances for this property? yes \_\_\_\_\_ no X

12. Has a Special Permit previously been granted for this property? yes \_\_\_\_\_ no X

IF THIS APPLICATION IS SIGNED BY ANYONE OTHER THAN THE PROPERTY OWNER, A SEPARATE NOTARIZED STATEMENT OR PROXY STATEMENT FROM THE OWNER MUST BE SUBMITTED, AT THE TIME OF APPLICATION, AUTHORIZING THIS APPLICATION.

STATE OF NEW YORK)

SS.:

COUNTY OF ORANGE)

THE UNDERSIGNED APPLICANT, BEING DULY SWORN, DEPOSES AND STATES THAT THE INFORMATION, STATEMENTS AND REPRESENTATIONS CONTAINED IN THIS APPLICATION AND SUPPORTING DOCUMENTS AND DRAWINGS ARE TRUE AND ACCURATE TO THE BEST OF HIS/HER KNOWLEDGE AND/OR BELIEF. THE APPLICANT FURTHER ACKNOWLEDGES RESPONSIBILITY TO THE TOWN FOR ALL FEES AND COSTS ASSOCIATED WITH THE REVIEW OF THIS APPLICATION.

SWORN BEFORE ME THIS:

*[Signature]*  
(OWNER'S SIGNATURE)

27<sup>th</sup> DAY OF November 2006

(AGENT'S SIGNATURE)

ARCHANA THAKAR

NOTARY PUBLIC OF NEW JERSEY

MY COMMISSION EXPIRES SEPT. 30, 2009

Please Print Agent's Name as Signed

*[Signature]*

NOTARY PUBLIC

TOWN USE ONLY:

DATE APPLICATION RECEIVED

APPLICATION NUMBER

RECEIVED  
TOWN OF NEW WINDSOR  
NOV 30 2006  
ENGINEER & PLANNING

06-32

**AGENT/OWNER PROXY STATEMENT**  
*(for professional representation)*

for submittal to the:  
**TOWN OF NEW WINDSOR PLANNING BOARD**

Jeffrey Kessler deposes and says that he resides  
(OWNER)  
at 625 Navaho Trail Dr., Franklin, NJ 07417 in the County of Bergen  
(OWNER'S ADDRESS)  
and State of New Jersey and that he is the owner of property tax map  
(Sec. 69 Block 4 Lot 3)  
designation number (Sec. 69 Block 4 Lot 3) which is the premises described in  
the foregoing application and that he designates:

(Agent Name & Address)  
Thomas M. DePuy, T.M. DePuy Engineering & Land Surveying, P.C.  
2656 Route 302, Middletown, New York 10941  
(Name & Address of Professional Representative of Owner and/or Agent)

as his agent to make the attached application.

**THIS DESIGNATION SHALL BE EFFECTIVE UNTIL WITHDRAWN BY THE OWNER OR UNTIL TWO (2) YEARS FROM THE DATE AGREED TO, WHICH EVER IS SOONER.**

SWORN BEFORE ME THIS:

27<sup>th</sup> DAY OF November 2006

ARCHANA THAKAR  
NOTARY PUBLIC OF NEW JERSEY  
MY COMMISSION EXPIRES SEPT. 30, 2009

AK-Thakar  
NOTARY PUBLIC

\*\* Jeffrey Kessler  
Owner's Signature (MUST BE NOTARIZED)

Agent's Signature (If Applicable)

Professional Representative's Signature

**\*\* PLEASE NOTE: ONLY OWNER'S SIGNATURE MUST BE NOTARIZED.**

**THIS PROXY SHALL BE VOID TWO (2) YEARS AFTER AGREED TO BY THE OWNER**

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TOWN OF NEW WINDSOR  
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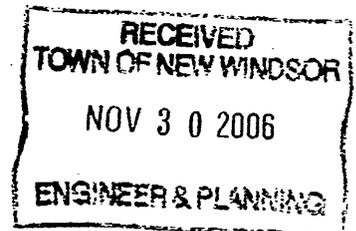
06-32

**TOWN OF NEW WINDSOR PLANNING BOARD**

**SITE PLAN CHECKLIST**

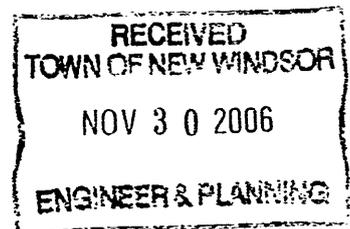
**ITEM**

- 1. X Site Plan Title
  - 2. — Provide 4" wide X 2" high box (**IN THE LOWEST RIGHT CORNER OF THE PLAN**) for use by Planning Board in affixing Stamp of Approval. (ON ALL PAGES OF SITE PLAN). *(REQUIRES COOR WITH TOWN OF CORNWALL)*
- SAMPLE:** 
- 3. X Applicant's Name(s)
  - 4. X Applicant's Address
  - 5. X Site Plan Preparer's Name
  - 6. X Site Plan Preparer's Address
  - 7. X Drawing Date
  - 8. X Revision Dates
  - 9. X Area Map Inset and Site Designation
  - 10. X Properties within 500' of site
  - 11. X Property Owners (Item #10)
  - 12. X Plot Plan
  - 13. X Scale (1" = 50' or lesser)
  - 14. X Metes and Bounds
  - 15. X Zoning Designation
  - 16. X North Arrow
  - 17. X Abutting Property Owners
  - 18. X Existing Building Locations
  - 19. X Existing Paved Areas
  - 20. X Existing Vegetation
  - 21. X Existing Access & Egress



**PROPOSED IMPROVEMENTS**

- 22.   X   Landscaping
- 23.   X   Exterior Lighting
- 24.   X   Screening
- 25.   X   Access & Egress
- 26.   X   Parking Areas
- 27.   X   Loading Areas
- 28.   X   Paving Details (Items 25 - 27)
- 29.   X   Curbing Locations
- 30.   X   Curbing through section
- 31.   X   Catch Basin Locations
- 32.   X   Catch Basin Through Section
- 33.   X   Storm Drainage
- 34.   —   Refuse Storage
- 35.   —   Other Outdoor Storage
- 36.   X   Water Supply
- 37.   X   Sanitary Disposal System
- 38.   X   Fire Hydrants
- 39.   X   Building Locations
- 40.   X   Building Setbacks
- 41.   X   Front Building Elevations
- 42.   X   Divisions of Occupancy
- 43.   PENDING   Sign Details
- 44.   X   Bulk Table Inset
- 45.   X   Property Area (Nearest 100 sq. ft.)
- 46.   X   Building Coverage (sq. ft.)
- 47.   X   Building Coverage (% of total area)
- 48.   ~~X~~   Pavement Coverage (sq. ft.) (*PENDING INT*)
- 49.   ~~X~~   Pavement Coverage (% of total area) (*PENDING INT*)
- 50.   —   Open Space (sq. ft.)
- 51.   —   Open Space (% of total area)
- 52.   X   No. of parking spaces proposed
- 53.   X   No. of parking spaces required



REFERRING TO QUESTION 9 ON THE APPLICATION FORM, AIS THIS PROPERTY WITHIN AN AGRICULTURAL DISTRICT CONTAINING A FARM OPERATION OR WITHIN 500 FEET OF A FARM OPERATION LOCATED IN AN AGRICULTURAL DISTRICT, PLEASE NOTE THE FOLLOWING:

- 54. N/A Referral to Orange County Planning Dept. is required for all applicants filing AD Statement.
- 55. PENDING A disclosure Statement, in the form set below, must be inscribed on all site plan maps prior to the affixing of a stamp of approval, whether or not the Planning Board specifically requires such a statement as a condition of approval.

APrior to the sale, lease, purchase, or exchange of property on this site which is wholly or partially within or immediately adjacent to or within 500 feet of a farm operation, the purchaser or leasee shall be notified of such farm operation with a copy of the following notification.

It is the policy of this State and this community to conserve, protect and encourage the development and improvement of agricultural land for the production of food, and other products, and also for its natural and ecological value. This notice is to inform prospective residents that the property they are about to acquire lies partially or wholly within an agricultural district or within 500 feet of such a district and that farming activities occur within the district. Such farming activities may include, but not be limited to, activities that cause noise, dust and odors.

This list is provided as a guide only and is for the convenience of the Applicant. The Town of New Windsor Planning Board may require additional notes or revisions prior to granting approval.

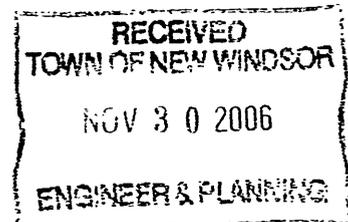
**PREPARER'S ACKNOWLEDGMENT:**

THE PLAT FOR THE PROPOSED SITE PLAN HAS BEEN PREPARED IN ACCORDANCE WITH THIS CHECKLIST AND THE TOWN OF NEW WINDSOR ORDINANCES, TO THE BEST OF MY KNOWLEDGE.

BY [Signature] 11/27/06  
 Licensed Professional Date

⌘ ⌘ ⌘ ⌘ ⌘ ⌘ PLEASE NOTE: ⌘ ⌘ ⌘ ⌘ ⌘ ⌘

**THE APPLICANT OR THEIR REPRESENTATIVE IS RESPONSIBLE TO KEEP TRACK OF ALL EXPIRATION DATES FOR ANY AND ALL APPROVALS GRANTED TO A PROJECT. EXTENSIONS MUST BE APPLIED FOR PRIOR TO EXPIRATION DATE.**



06-02

**State Environmental Quality Review  
FULL ENVIRONMENTAL ASSESSMENT FORM**

**Purpose:** The full EAF is designed to help applicants and agencies determine, in an orderly manner, whether a project or action may be significant. The question of whether an action may be significant is not always easy to answer. Frequently, there are aspects of a project that are subjective or unmeasurable. It is also understood that those who determine significance may have little or no formal knowledge of the environment or may not be technically expert in environmental analysis. In addition, many who have knowledge in one particular area may not be aware of the broader concerns affecting the question of significance.

The full EAF is intended to provide a method whereby applicants and agencies can be assured that the determination process has been orderly, comprehensive in nature, yet flexible enough to allow introduction of information to fit a project or action.

**Full EAF Components:** The full EAF is comprised of three parts:

- Part 1:** Provides objective data and information about a given project and its site. By identifying basic project data, it assists a reviewer in the analysis that takes place in Parts 2 and 3.
- Part 2:** Focuses on identifying the range of possible impacts that may occur from a project or action. It provides guidance as to whether an impact is likely to be considered small to moderate or whether it is a potentially-large impact. The form also identifies whether an impact can be mitigated or reduced.
- Part 3:** If any impact in Part 2 is identified as potentially-large, then Part 3 is used to evaluate whether or not the impact is actually important.

**DETERMINATION OF SIGNIFICANCE— Type 1 and Unlisted Actions**

**Identify the Portions of EAF completed for this project:**     Part 1     Part 2     Part 3

Upon review of the information recorded on this EAF (Parts 1 and 2 and 3 if appropriate), and any other supporting information, and considering both the magnitude and importance of each impact, it is reasonably determined by the lead agency that:

- A. The project will not result in any large and important impact(s) and, therefore, is one which **will not** have a significant impact on the environment, therefore a **negative declaration will be prepared.**
- B. Although the project could have a significant effect on the environment, there will not be a significant effect for this Unlisted Action because the mitigation measures described in PART 3 have been required, therefore a **CONDITIONED negative declaration will be prepared.\***
- C. The project may result in one or more large and important impacts that may have a significant impact on the environment, therefore a **positive declaration will be prepared.**

\* A Conditioned Negative Declaration is only valid for Unlisted Actions

VAILS GATE BUSINESS PARK

Name of Action

Town of Cornwall Planning Board

Name of Lead Agency

Print or Type Name of Responsible Officer in Lead Agency

Title of Responsible Officer

Signature of Responsible Officer in Lead Agency

Signature of Preparer (If different from responsible officer)

Date

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# PART 1—PROJECT INFORMATION

## Prepared by Project Sponsor

NOTICE: This document is designed to assist in determining whether the action proposed may have a significant effect on the environment. Please complete the entire form. Parts A through E. Answers to these questions will be considered as part of the application for approval and may be subject to further verification and public review. Provide any additional information you believe will be needed to complete Parts 2 and 3.

It is expected that completion of the full EAF will be dependent on information currently available and will not involve new studies, research or investigation. If information requiring such additional work is unavailable, so indicate and specify each instance.

NAME OF ACTION <b>Vails Gate Business Park</b>			
LOCATION OF ACTION (include Street Address, Municipality and County) <b>1215 Route 94, New Windsor, NY 12553</b>			
NAME OF APPLICANT/SPONSOR <b>Vails Gate Business Center LLC</b>		BUSINESS TELEPHONE <b>(973) 340-0202</b>	
ADDRESS <b>460 Getty Avenue</b>			
CITY/PO <b>Clifton</b>		STATE <b>NJ</b>	ZIP CODE <b>07011</b>
NAME OF OWNER (if different)		BUSINESS TELEPHONE <b>( )</b>	
ADDRESS			
CITY/PO		STATE	ZIP CODE
DESCRIPTION OF ACTION <b>Redevelopment of three industrial/commerical buildings (the former Tarkett facility) for General Manufacturing Industrial Processing in Buildings 1 and 3 and self storage in Building 2, located in the Town of Cornwall(4-3-1.1) and Town of New Windsor (69-4-3) on 35.9 acre parcel.</b>			

Please Complete Each Question--Indicate N.A. if not applicable

### A. Site Description

Physical setting of overall project, both developed and undeveloped areas.

1. Present land use:     Urban     Industrial     Commercial     Residential (suburban)     Rural (non-farm)  
                                   Forest     Agriculture     Other \_\_\_\_\_

2. Total acreage of project area:    35.9 acres.

APPROXIMATE ACREAGE	PRESENTLY	AFTER COMPLETION
Meadow or Brushland (Non-agricultural)	<u>8.4</u> acres	<u>8.8</u> acres
Forested	<u>7.2</u> acres	<u>7.2</u> acres
Agricultural (Includes orchards, cropland, pasture, etc.)	<u>0</u> acres	<u>0</u> acres
Wetland (Freshwater or tidal as per Articles 24, 25 of ECL)	<u>5.8</u> acres	<u>5.9</u> acres (1)
Water Surface Area	<u>(1), (2)</u> acres	<u>(1), (2)</u> acres
Unvegetated (Rock, earth or fill)	<u>0.5</u> acres	<u>0.5</u> acres
Roads, buildings and other paved surfaces	<u>13.4</u> acres	<u>13.1</u> acres
Other (Indicate type) <u>Federal wetland (delineated)</u>	<u>0.5</u> acres	<u>0.4</u> acres (2)

3. What is predominant soil type(s) on project site?    Fill

- a. Soil drainage:     Well drained \_\_\_\_\_ % of site     Moderately well drained 100 % of site  
                                   Poorly drained \_\_\_\_\_ % of site

- b. If any agricultural land is involved, how many acres of soil are classified within soil group 1 through 4 of the NYS Land Classification System?    n/a acres. (See 1 NYCRR 370). (1) includes 2.6 acres open water

4. Are there rock outcroppings on project site?     Yes     No (2) includes 0.1 acres wetlands

- a. What is depth to bedrock? \_\_\_\_\_ (in feet)

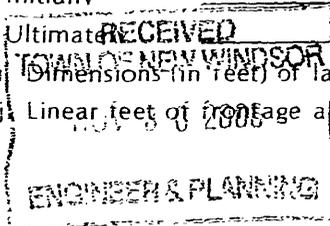
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**ENGINEER & PLANNING**

06-32

5. Approximate percentage of proposed project site with slopes:  0-10% \_\_\_\_\_ %  10-15% 10 %  
 15% or greater \_\_\_\_\_ %
6. Is project substantially contiguous to, or contain a building, site, or district, listed on the State or the National Registers of Historic Places?  Yes  No
7. Is project substantially contiguous to a site listed on the Register of National Natural Landmarks?  Yes  No
8. What is the depth of the water table? 2 (in feet)
9. Is site located over a primary, principal, or sole source aquifer?  Yes  No
10. Do hunting, fishing or shell fishing opportunities presently exist in the project area?  Yes  No
11. Does project site contain any species of plant or animal life that is identified as threatened or endangered?  
 Yes  No According to T.M. DePuy  
 Identify each species \_\_\_\_\_
12. Are there any unique or unusual land forms on the project site? (i.e., cliffs, dunes, other geological formations)  
 Yes  No Describe T.M. DePuy
13. Is the project site presently used by the community or neighborhood as an open space or recreation area?  
 Yes  No If yes, explain \_\_\_\_\_
14. Does the present site include scenic views known to be important to the community?  
 Yes  No
15. Streams within or contiguous to project area: tributary to Moodna Creek H-89  
 a. Name of Stream and name of River to which it is tributary Moodna Creek
16. Lakes, ponds, wetland areas within or contiguous to project area:  
 a. Name no name b. Size (In acres) \_\_\_\_\_
17. Is the site served by existing public utilities?  Yes  No  
 a) If Yes, does sufficient capacity exist to allow connection?  Yes  No  
 b) If Yes, will improvements be necessary to allow connection?  Yes  No
18. Is the site located in an agricultural district certified pursuant to Agriculture and Markets Law, Article 25-AA, Section 303 and 304?  Yes  No
19. Is the site located in or substantially contiguous to a Critical Environmental Area designated pursuant to Article 8 of the ECL, and 6 NYCRR 617?  Yes  No
20. Has the site ever been used for the disposal of solid or hazardous wastes?  Yes  No

## B. Project Description

1. Physical dimensions and scale of project (fill in dimensions as appropriate)
- a. Total contiguous acreage owned or controlled by project sponsor 35.9 acres.
- b. Project acreage to be developed: 13.4 acres initially; 13.1 acres ultimately.
- c. Project acreage to remain undeveloped 22.8 acres.
- d. Length of project, in miles: n/a (If appropriate)
- e. If the project is an expansion, indicate percent of expansion proposed n/a (Redevelopment) %;
- f. Number of off-street parking spaces existing 220; proposed 220.
- g. Maximum vehicular trips generated per hour 106 (upon completion of project)? weekday peak pm hour
- h. If residential: Number and type of housing units: N/A
- |            | One Family | Two Family | Multiple Family | Condominium |
|------------|------------|------------|-----------------|-------------|
| Initially  | _____      | _____      | _____           | _____       |
| Ultimately | _____      | _____      | _____           | _____       |
- i. Dimensions (in feet) of largest ~~proposed~~ existing structure 27 height; 352 width; 622 length.
- j. Linear feet of frontage along a public thoroughfare project will occupy is? 58.3 ft.



2. How much natural material (i.e., rock, earth, etc.) will be removed from the site? 0 tons/cubic yards
3. Will disturbed areas be reclaimed?  Yes  No  N/A
  - a. If yes, for what intended purpose is the site being reclaimed? Grass area
  - b. Will topsoil be stockpiled for reclamation?  Yes  No
  - c. Will upper subsoil be stockpiled for reclamation?  Yes  No
4. How many acres of vegetation (trees, shrubs, ground covers) will be removed from site? \*see below acres.
5. Will any mature forest (over 100 years old) or other locally-important vegetation be removed by this project?  Yes  No
6. If single phase project: Anticipated period of construction 12 months, (including demolition).
7. If multi-phased: N/A
  - a. Total number of phases anticipated \_\_\_\_\_ (number).
  - b. Anticipated date of commencement phase 1 \_\_\_\_\_ month \_\_\_\_\_ year, (including demolition).
  - c. Approximate completion date of final phase \_\_\_\_\_ month \_\_\_\_\_ year.
  - d. Is phase 1 functionally dependent on subsequent phases?  Yes  No
8. Will blasting occur during construction?  Yes  No
9. Number of jobs generated: during construction 30; after project is complete 50
10. Number of jobs eliminated by this project 0
11. Will project require relocation of any projects or facilities?  Yes  No If yes, explain \_\_\_\_\_
12. Is surface liquid waste disposal involved?  Yes  No
  - a. If yes, indicate type of waste (sewage, industrial, etc.) and amount 2,240 GPD
  - b. Name of water body into which effluent will be discharged T. New Windsor Sewer System
13. Is subsurface liquid waste disposal involved?  Yes  No Type \_\_\_\_\_
14. Will surface area of an existing water body increase or decrease by proposal?  Yes  No  
Explain \_\_\_\_\_
15. Is project or any portion of project located in a 100 year flood plain?  Yes  No
16. Will the project generate solid waste?  Yes  No
  - a. If yes, what is the amount per month 6 tons
  - b. If yes, will an existing solid waste facility be used?  Yes  No
  - c. If yes, give name Dunmore; location Dunmore, PA
  - d. Will any wastes **not** go into a sewage disposal system or into a sanitary landfill?  Yes  No
  - e. If Yes, explain \_\_\_\_\_
17. Will the project involve the disposal of solid waste?  Yes  No
  - a. If yes, what is the anticipated rate of disposal? \_\_\_\_\_ tons/month.
  - b. If yes, what is the anticipated site life? \_\_\_\_\_ years.
18. Will project use herbicides or pesticides?  Yes  No
19. Will project routinely produce odors (more than one hour per day)?  Yes  No
20. Will project produce operating noise exceeding the local ambient noise levels?  Yes  No
21. Will project result in an increase in energy use?  Yes  No  
If yes, indicate type(s) Electric
22. If water supply is from wells, indicate pumping capacity n/a gallons/minute.
23. Total anticipated water usage per day 2,240 gallons/day.
24. Does project involve Local, State or Federal funding?  Yes  No  
If Yes, explain \_\_\_\_\_

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\*Redevelopment will result in less impervious & an increase of ground cover.

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25. Approvals Required:

		Type	Submittal Date
City, Town, Village Board	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		
City, Town, Village Planning Board	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	T. Cornwall & New Windsor	
City, Town Zoning Board	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		
City, County Health Department	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		
Other Local Agencies	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		
Other Regional Agencies	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		
State Agencies	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	NYSDEC Wetland Buffer	
Federal Agencies	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	ACE permit may be required	

**C. Zoning and Planning Information**

- Does proposed action involve a planning or zoning decision?  Yes  No  
 If Yes, indicate decision required:  
 zoning amendment    zoning variance    special use permit    subdivision    site plan  
 new/revision of master plan    resource management plan    other PID-Planned Industrial Office (Cornwall)
- What is the zoning classification(s) of the site? C - Design Shopping (New Windsor)
- What is the maximum potential development of the site if developed as permitted by the present zoning?  
n/a
- What is the proposed zoning of the site? n/a
- What is the maximum potential development of the site if developed as permitted by the proposed zoning?  
n/a
- Is the proposed action consistent with the recommended uses in adopted local land use plans?  Yes  No
- What are the predominant land use(s) and zoning classifications within a 1/4 mile radius of proposed action?  
Commercial / Industrial
- Is the proposed action compatible with adjoining/surrounding land uses within a 1/4 mile?  Yes  No
- If the proposed action is the subdivision of land, how many lots are proposed? n/a  
 a. What is the minimum lot size proposed? n/a
- Will proposed action require any authorization(s) for the formation of sewer or water districts?  Yes  No
- Will the proposed action create a demand for any community provided services (recreation, education, police, fire protection)?  Yes  No  
 a. If yes, is existing capacity sufficient to handle projected demand?  Yes  No
- Will the proposed action result in the generation of traffic significantly above present levels?  Yes  No  
 a. If yes, is the existing road network adequate to handle the additional traffic?  Yes  No  
 (traffic study submitted)

**D. Informational Details**

Attach any additional information as may be needed to clarify your project. If there are or may be adverse impacts associated with your proposal, please discuss such impacts and the measures which you propose to avoid or avoid them.

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**E. Verification**

I certify that the information provided above is true to the best of my knowledge.

Applicant/Sponsor Name [Signature] Date \_\_\_\_\_  
 Signature [Signature] Title President

If the action is in the Coastal Area, and you are a state agency, complete the Coastal Assessment Form before proceeding with this assessment.

## Part 2—PROJECT IMPACTS AND THEIR MAGNITUDE

Responsibility of Lead Agency

**General Information (Read Carefully)**

- In completing the form the reviewer should be guided by the question: Have my responses and determinations been **reasonable**? The reviewer is not expected to be an expert environmental analyst.
- The **Examples** provided are to assist the reviewer by showing types of impacts and wherever possible the threshold of magnitude that would trigger a response in column 2. The examples are generally applicable throughout the State and for most situations. But, for any specific project or site other examples and/or lower thresholds may be appropriate for a Potential Large Impact response, thus requiring evaluation in Part 3.
- The impacts of each project, on each site, in each locality, will vary. Therefore, the examples are illustrative and have been offered as guidance. They do not constitute an exhaustive list of impacts and thresholds to answer each question.
- The number of examples per question does not indicate the importance of each question.
- In identifying impacts, consider long term, short term and cumulative effects.

**Instructions (Read carefully)**

- a. Answer each of the 20 questions in PART 2. Answer **Yes** if there will be **any** impact.
- b. **Maybe** answers should be considered as **Yes** answers.
- c. If answering **Yes** to a question then check the appropriate box (column 1 or 2) to indicate the potential size of the impact. If impact threshold equals or exceeds any example provided, check column 2. If impact will occur but threshold is lower than example, check column 1.
- d. Identifying that an impact will be potentially large (column 2) does not mean that it is also necessarily **significant**. Any large impact must be evaluated in PART 3 to determine significance. Identifying an impact in column 2 simply asks that it be looked at further.
- e. If reviewer has doubt about size of the impact then consider the impact as potentially large and proceed to PART 3.
- f. If a potentially large impact checked in column 2 can be mitigated by change(s) in the project to a small to moderate impact, also check the **Yes** box in column 3. A **No** response indicates that such a reduction is not possible. This must be explained in Part 3.

### IMPACT ON LAND

1. Will the proposed action result in a physical change to the project site?  
 NO     YES

**Examples** that would apply to column 2

- Any construction on slopes of 15% or greater, (15 foot rise per 100 foot of length), or where the general slopes in the project area exceed 10%.
- Construction on land where the depth to the water table is less than 3 feet.
- Construction of paved parking area for 1,000 or more vehicles.
- Construction on land where bedrock is exposed or generally within 3 feet of existing ground surface.
- Construction that will continue for more than 1 year or involve more than one phase or stage.
- Excavation for mining purposes that would remove more than 1,000 tons of natural material (i.e., rock or soil) per year.
- Construction or expansion of a sanitary landfill.
- Construction in a designated floodway.
- Other impacts \_\_\_\_\_

2. Will there be an effect to any unique or unusual land forms found on the site? (e.g., dunes, geological formations, etc.)  NO     YES

• Specific land forms: \_\_\_\_\_

1 Small to Moderate Impact	2 Potential Large Impact	3 Can Impact Be Mitigated By Project Change
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No

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**IMPACT ON WATER**

3 Will proposed action affect any water body designated as protected? (Under Articles 15, 24, 25 of the Environmental Conservation Law, ECL) NO YES

Examples that would apply to column 2

- Developable area of site contains a protected water body.
- Dredging more than 100 cubic yards of material from channel of a protected stream.
- Extension of utility distribution facilities through a protected water body.
- Construction in a designated freshwater or tidal wetland.
- Other impacts: \_\_\_\_\_

4 Will proposed action affect any non-protected existing or new body of water? NO YES

Examples that would apply to column 2

- A 10% increase or decrease in the surface area of any body of water or more than a 10 acre increase or decrease.
- Construction of a body of water that exceeds 10 acres of surface area.
- Other impacts: \_\_\_\_\_

5 Will Proposed Action affect surface or groundwater quality or quantity? NO YES

Examples that would apply to column 2

- Proposed Action will require a discharge permit.
- Proposed Action requires use of a source of water that does not have approval to serve proposed (project) action.
- Proposed Action requires water supply from wells with greater than 45 gallons per minute pumping capacity.
- Construction or operation causing any contamination of a water supply system.
- Proposed Action will adversely affect groundwater.
- Liquid effluent will be conveyed off the site to facilities which presently do not exist or have inadequate capacity.
- Proposed Action would use water in excess of 20,000 gallons per day.
- Proposed Action will likely cause siltation or other discharge into an existing body of water to the extent that there will be an obvious visual contrast to natural conditions.
- Proposed Action will require the storage of petroleum or chemical products greater than 1,100 gallons.
- Proposed Action will allow residential uses in areas without water and/or sewer services.
- Proposed Action locates commercial and/or industrial uses which may require new or expansion of existing waste treatment and/or storage facilities.
- Other impacts: \_\_\_\_\_

6 Will proposed action alter drainage flow or patterns, or surface water flow? NO YES

Examples that would apply to column 2

- Proposed action would change flood water flows.

1 Small to Moderate Impact	2 Potential Large Impact	3 Can Impact Be Mitigated By Project Change	
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes	<input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes	<input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes	<input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes	<input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes	<input type="checkbox"/> No
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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes	<input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes	<input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes	<input type="checkbox"/> No

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**IMPACT ON CRITICAL ENVIRONMENTAL AREAS**

14. Will Proposed Action impact the exceptional or unique characteristics of a critical environmental area (CEA) established pursuant to subdivision 6 NYCRR 617.14(g)?  NO  YES  
 List the environmental characteristics that caused the designation of the CEA.

\_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

Examples that would apply to column 2

- Proposed Action to locate within the CEA?
- Proposed Action will result in a reduction in the quantity of the resource?
- Proposed Action will result in a reduction in the quality of the resource?
- Proposed Action will impact the use, function or enjoyment of the resource?
- Other impacts: \_\_\_\_\_

**IMPACT ON TRANSPORTATION**

15. Will there be an effect to existing transportation systems?  NO  YES

Examples that would apply to column 2

- Alteration of present patterns of movement of people and/or goods.
- Proposed Action will result in major traffic problems.
- Other impacts: \_\_\_\_\_

**IMPACT ON ENERGY**

16. Will proposed action affect the community's sources of fuel or energy supply?  NO  YES

Examples that would apply to column 2

- Proposed Action will cause a greater than 5% increase in the use of any form of energy in the municipality.
- Proposed Action will require the creation or extension of an energy transmission or supply system to serve more than 50 single or two family residences or to serve major commercial or industrial use.
- Other impacts: \_\_\_\_\_

1 Small to Moderate Impact	2 Potential Large Impact	3 Can Impact Be Mitigated By Project Change
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Yes <input type="checkbox"/> No

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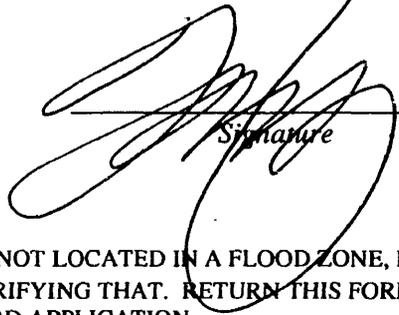
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ATTACHMENTS

- A. Flood Hazard Area Development Permit Application Form.
- B. Certificate of Compliance

I Thomas M. DePuy hereby certify that the property or properties mentioned in this application is/are not/located in a flood zone.

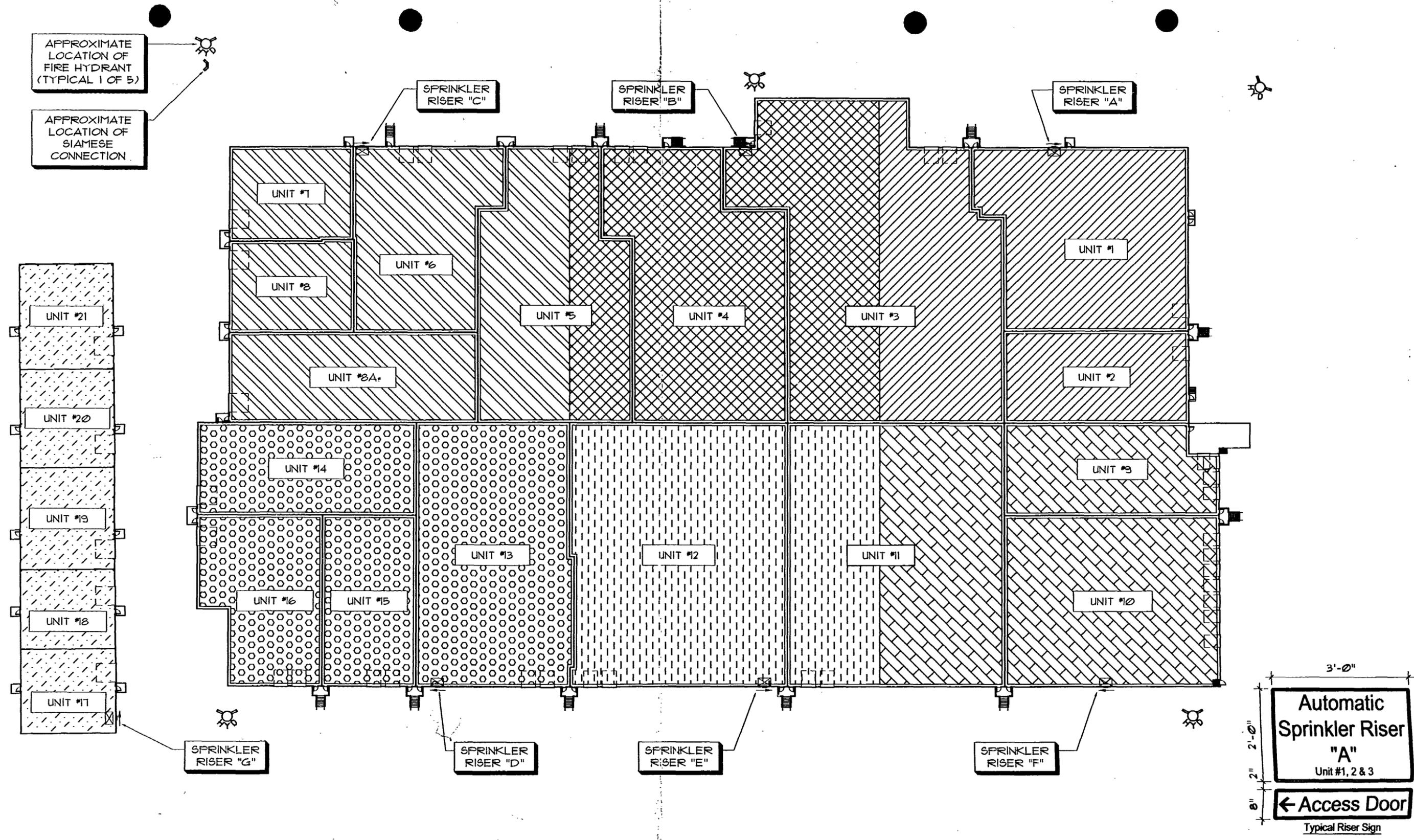
  
Signature

PLEASE NOTE: \* IF PROPERTY IS NOT LOCATED IN A FLOOD ZONE, PLEASE SIGN ABOVE VERIFYING THAT. RETURN THIS FORM WITH PLANNING BOARD APPLICATION

IF PROPERTY IS LOCATED IN A FLOOD ZONE, PLEASE COMPLETE THE ATTACHED PAPERS AND RETURN SAME WITH PLANNING BOARD APPLICATION.

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APPROXIMATE LOCATION OF FIRE HYDRANT (TYPICAL 1 OF 5)

APPROXIMATE LOCATION OF SIAMESE CONNECTION

SPRINKLER RISER "C"

SPRINKLER RISER "B"

SPRINKLER RISER "A"

UNIT #1

UNIT #6

UNIT #1

UNIT #8

UNIT #5

UNIT #4

UNIT #3

UNIT #8A

UNIT #2

UNIT #21

UNIT #20

UNIT #14

UNIT #9

UNIT #19

UNIT #13

UNIT #12

UNIT #11

UNIT #16

UNIT #15

UNIT #10

UNIT #18

UNIT #17

SPRINKLER RISER "G"

SPRINKLER RISER "D"

SPRINKLER RISER "E"

SPRINKLER RISER "F"

3'-0"  
2'-0"  
Automatic Sprinkler Riser "A"  
Unit #1, 2 & 3

← Access Door  
Typical Riser Sign

Scale: 1" = 60'

# Vails Gate Business Center - Sprinkler Riser Areas

Vails Gate, New York 12584

DWG	TITLE
SP1	BOUNDARY AND ZONING INFORMATION
SP2	EXISTING CONDITIONS PLAN
SP3	SITE PLAN A
SP4	SITE PLAN B
SP5	PAVEMENT MARKING AND SIGN PLAN
SP6	SITE LIGHTING PLAN & DETAILS
SP7	SITE & UTILITY DETAILS
SP8	STORM WATER LIFT STATION AND POND OUTLETS
SP9	EXISTING FIRE PROTECTION SYSTEM
RFT 1	R F I OVERLAY PLAN
REFERENCE DRAWINGS	
ES1	SWPP NOTES AND DETAILS
ES2	SOIL EROSION AND SEDIMENTATION CONTROL PLAN
ES3	WATER QUALITY POND 1 - PLAN AND LANDSCAPING
ES4	WATER QUALITY POND 2 - PLAN AND LANDSCAPING

LEGEND		ABBREVIATIONS	
SYMBOLS			
	PROPERTY LINE	CB	CATCH BASIN
	ADJOINING LOT LINE	CD/P	PERF. SEPTIC CURTAIN DRAIN PIPE
	EASEMENT LINE	CD/S	SOLID SEPTIC CURTAIN DRAIN PIPE
	CONTOURS	CL	CENTERLINE
	WATER EDGE OR STREAM	CM/P	CORRUGATED METAL PIPE
	GUIDE RAIL	C.O.	CLEAN OUT
	FENCE LINE	CONC.	CONCRETE
	STONE WALL	D.I.P.	DUCTILE IRON PIPE
	EDGE OF PAVEMENT	ELEVATION	ELEVATION
	GAS MAIN	FFE	FINISHED FLOOR ELEVATION
	OVERHEAD UTILITY LINE	FP	FLAG POLE (EXIST)
	TRESTLE	GV	GAS VALVE
	ROAD SIGN	G.F.A.	GROSS FLOOR AREA
	UTILITY POLE	HDPE	HIGH DENSITY POLYETHYLENE (SMOOTH INTERIOR)
	WELL	HP	HIGH POINT
	WATERMAIN GATE VALVE	INV	INVERT
	HYDRANT (EXIST)	LF	LINEAR FOOT
	RAILROAD TRACK	LP	LOW POINT
	DIVERSION SWALE	LSE	LOWEST SEWER ELEVATION (NV)
	AREA LIGHT POLE	M	MANHOLE
	ROOF/FOOTING DRAINS	MW	MONITORING WELL
	WETLAND BOUNDARY LINE	N.W.L.	NORMAL WATER LEVEL (SURFACE ELEV)
	WETLAND BUFFER LINE (100 FT)	O.A.	OVER ALL (DISTANCE)
	WETLAND AREA	OE	OVERHEAD ELECTRIC
	CONCRETE SURFACES	OHW	OVERHEAD WIRES
	PROPOSED SURFACES	PROP.	PROPOSED
	PROPOSED BITUMINOUS SURFACES	RCP	REINFORCED CONCRETE PIPE
	BITUMINOUS SURFACES (TO BE REMOVED)	WL	WATER LEVEL (SURFACE ELEV)
		WV	WATER VALVE

**SURVEY AND PARCEL INFORMATION NOTES:**

- MAP REFERENCE: TARKETT, INC. MAINTENANCE SHOP ADDITION, TOWNS OF CORNWALL AND NEW WINDSOR, ORANGE COUNTY, N.Y. SITE PLAN DATED JANUARY 21, 1992, PREPARED BY PANY & LENTZ ENGINEERING COMPANY.
- MAP REFERENCE: "SURVEY OF PROPERTY, KESSLER PROPERTIES, SECTION 69, BLOCK 4 LOT 3 (NEW WINDSOR), SECTION 4, BLOCK 3 LOT 11 (CORNWALL), COUNTY OF ORANGE, NEW YORK" DATED JULY 31, 2006, PREPARED BY HERITAGE LAND SURVEYING, P.C.
- SUBJECT TO TELEPHONE LINE EASEMENT TO HUDSON RIVER TELEPHONE COMPANY DESCRIBED AS BEING LOCATED ALONG HIGHWAY KNOWN AS THE VALS GATE AND CHESTER STATE ROAD AS DESCRIBED IN LIBER 524 OF DEEDS, PAGE 5.
- SUBJECT TO CENTRAL HUDSON GAS & ELECTRIC CORP. AND CORNWALL TELEPHONE COMPANY GRANTED EASEMENT AND RIGHT-OF-WAY ACROSS LANDS OF POPOWICK AS DESCRIBED IN LIBER 1031 OF DEEDS, PAGE 274.
- SUBJECT TO EASEMENT AND RIGHT-OF-WAY OVER PROPERTY DESCRIBED IN LIBER 1707 OF DEEDS, PAGE 334 FOR USE BY JOSEPH & MARY MYZELOW, THEIR HEIRS AND ASSIGNS FOREVER.
- SUBJECT TO AN EASEMENT AND RIGHT-OF-WAY SECURED BY RUBBEROID COMPANY AS DESCRIBED IN LIBER 1716 OF DEEDS, PAGE 736.
- SUBJECT TO ELECTRIC AND GAS TRANSMISSION LINES EASEMENT OPTION TO CENTRAL HUDSON GAS & ELECTRIC CORP. FOR PURCHASE OF 40' WIDE EASEMENT ACROSS LANDS AS DESCRIBED IN LIBER 1136 OF DEEDS, PAGE 206.
- SUBJECT TO RIGHT TO CONSTRUCT, MAINTAIN, ETC. BY CENTRAL HUDSON GAS & ELECTRIC CORP. ACROSS LANDS OF JOHN H. WELLS AS DESCRIBED IN LIBER 558 OF DEEDS, PAGE 401.
- SUBJECT TO A PERMANENT EASEMENT AND RIGHT-OF-WAY 20' WIDE THROUGHOUT LANDS OF MUNDAY BY CENTRAL HUDSON GAS & ELECTRIC CORP. AS DESCRIBED IN 1326 OF DEEDS, PAGE 258.
- SUBJECT TO RIGHT-OF-WAY AND EASEMENT TO CONSTRUCT AND MAINTAIN AN ASPHALT ROAD NOT LESS THAN 20' WIDE OVER EASEMENT OF LIBER 1336 OF DEEDS, PAGE 208 AS DESCRIBED IN LIBER 1408 OF DEEDS, PAGE 44.
- SUBJECT TO ELECTRIC & GAS TRANSMISSION LINES EASEMENT OPTION TO CENTRAL HUDSON GAS & ELECTRIC CORP. FOR PURCHASE OF 40' WIDE EASEMENT ACROSS LANDS AS DESCRIBED IN LIBER 1136 OF DEEDS, PAGE 202.
- EXISTING FEDERAL AND HYSDOC WETLANDS SHOWN ARE BASED UPON ORANGE COUNTY WATER AUTHORITY MAPPING DATA CD'S AND ARE APPROXIMATE ONLY. ACTUAL LOCATIONS OF WETLANDS MAY BE SUBSTANTIALLY DIFFERENT THAN THOSE SHOWN AND SHOULD BE FIELD DELINEATED BY A WETLANDS SPECIALIST AND LOCATED BY A LICENSED LAND SURVEYOR. WETLANDS LABELED "A" AND "B" WERE DELINEATED BY MICHAEL NOWICKI OF ECOLOGICAL SOLUTIONS ON JULY 21, 2006.
- DATUM: NAVD 88 BENCHMARK DISK H322 PID: LY0301 ELEVATION OF DISK = 273.82 (LOCATION SHOWN ON SURVEY).
- SEVERAL AREAS OF THE PROPERTY WERE INACCESSIBLE DUE TO WATER CONDITIONS AT THE TIME OF SURVEY. NO VISIBLE SIGNS OF STRUCTURES WERE PRESENT AT THE TIME OF SURVEY.
- THE PROPERTY IS SUBJECT TO THE CONDITIONS CONTAINED IN AN UNRECORDED AGREEMENT DATED JANUARY 20, 1985 BETWEEN ERIE-LACKAWANNA RAILROAD COMPANY AND THE RUBBEROID COMPANY, RELATING TO THE CONSTRUCTION AND MAINTENANCE OF A RAILROAD SIDING (SEE LIBER 2206 OF DEEDS, PAGE 24 AND LIBER 3721 OF DEEDS, PAGE 12).

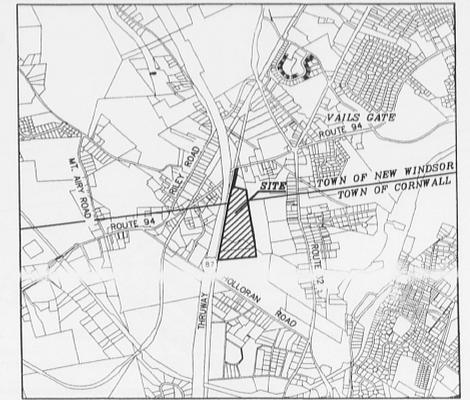
**PARKING REQUIREMENT COMPARISON TABLE**

TARKETT, INC. (PRIOR APPROVED USE)		VALS GATE BUSINESS CENTER (PROPOSED USE)	
MANUFACTURING FACILITY		COMBINATION WAREHOUSING AND LITE MANUFACTURING	
ZONING DISTRICT PIO - USE GROUP C	ZONING DISTRICT PIO - USE GROUP C	ZONING DISTRICT PIO - USE GROUP C	ZONING DISTRICT PIO - USE GROUP C
BUILDING AREA	~246,210 S.F.	BUILDING AREA	~246,210 S.F.
LIGHT MANUFACTURING (100%)		LIGHT MANUFACTURING (38%)	92,178 = 230 SPACES
246,210 S.F.		WAREHOUSING (62%)	150,390 = 150 SPACES = 384 SPACES
1 SPACE PER 400 S.F.	= 616 SPACES	MINI-STORAGE	3,644 = 4 SPACES
			1000 = 4 SPACES
SHIFT PARKING PROVIDED	110 SPACES PER SHIFT X 2 = 220 SPACES *	MAX. 8 HOUR PARKING DEMAND	100 SPACES X 2 = 200 SPACES
			PER 8 HR. PERIOD (230 SPACES PROVIDED)

(\* ) NUMBER OF PARKING SPACES APPROVED PURSUANT TO PRIOR SITE PLAN APPROVAL CITED IN NOTE NO. 1 ABOVE.

ITEM	TOWN OF CORNWALL - PLANNED INDUSTRIAL OFFICE (PIO)		TOWN OF NEW WINDSOR - DESIGN SHOPPING (D)	
	REQUIREMENTS	SITE	REQUIREMENTS	SITE
1. LOT AREA	80,000 S.F.	1,267,098 SF (29.1 AC)	40,000 S.F.	296,208 SF (6.8 AC)
2. LOT WIDTH	400 FEET	576.8 FEET	200 FEET	418.9 FEET
3. LOT DEPTH	-	1598.1 FEET	-	1137.1 FEET
4. DEVELOPMENT COVERAGE	70% = 207768 S.F.	-	85%	-
5. PROPOSED USE (BUILDING COVERAGE)		224,178 S.F.	N/A	-
o LIGHT MANUFACTURING WAREHOUSING				
BUILDING 2	35% MAX.	3,644 S.F. (PORTION)	N/R	10,564 S.F. (PORTION)
BUILDING 3		18,388 S.F.	N/A	-
o LIGHT MANUFACTURING WAREHOUSING		246,210 S.F.		10,564 S.F.
		1,297,396 S.F. X 100 = 19.4%		296,208 S.F. X 100 = 3.6%
6. LOADING BERTHS		32 BERTHS	N/R	-
BUILDING 1	N/R	-	N/R	-
BUILDING 2	N/R	-	N/R	-
BUILDING 3	N/R	5 BERTHS	N/R	-
		TOTAL = 37 BERTHS		
7. PARKING LOT		92,178 = 230 SPACES	230 SPACES PROVIDED (SEE COMPARISON TABLE AT RIGHT)	-
o LIGHT MANUFACTURING (38%)	400	150,390 = 150 SPACES	5 SPACES PER 10 STORAGE UNITS	7 SPACES (14 STORAGE UNITS)
WAREHOUSING (62%)	1000	3,644 = 4 SPACES		
MINI-STORAGE	1000			
8. BUILDING SETBACKS				
o FRONT YARD	BUILDING 1	100 FEET	0 FEET (NOTE 3)	60 FEET
	BUILDING 2			324.6 FEET
	BUILDING 3			-
o SIDE YARD	BUILDING 1	50 FEET	228.5 FEET	118.9 FEET
	BUILDING 2		203.3 FEET	-
	BUILDING 3		171.1 FEET	-
o SIDE YARD	BUILDING 1	50 FEET	95.4 FEET	-
	BUILDING 2		310.3 FEET	-
	BUILDING 3		400.4 FEET	-
o BOTH SIDE YARDS	BUILDING 1	100 FEET	323.9 FEET	-
	BUILDING 2		513.6 FEET	100 FEET
	BUILDING 3		571.5 FEET	435.3 FEET
o REAR YARD	BUILDING 1	50 FEET	601.48 FEET	-
	BUILDING 2		1497.5 FEET	-
	BUILDING 3		471.2 FEET	-
9. BUILDING HEIGHT				
	BUILDING 1	24 FEET	12" PER FOOT OF DISTANCE TO THE NEAREST LINE	13 FEET
	BUILDING 2	11 FEET		
	BUILDING 3	29.5 FEET	118.9 FEET	

- NOTES:
- PURSUANT TO SURVEY-PARCEL DATA NOTE 1 (MAP REFERENCE) SITE PLAN APPROVAL FOR 220 PARKING SPACES WAS GRANTED UNDER A PRE-EXISTING, NON-CONFORMING CONDITION.
  - TOTAL SPACES INDICATED ARE FOR ENTIRE PROPERTY, AND ARE NOT SEGREGATED BY MUNICIPALITY. (SEE PLAN SHEET 5 FOR BREAKDOWN)
  - BUILDING 2 STRADDLES THE CORNWALL/NEW WINDSOR TOWN LINE AND HAS A ZERO FOOT SETBACK WHEN THAT LINE IS CONSIDERED A FRONT OR REAR BOUNDARY.



**LOCATION MAP**  
SCALE: 1"=2000'

**PARCEL / OWNER DATA**

RECORD OWNER: VALS GATE BUSINESS CENTER, LLC  
1215 ROUTE 94  
NEW WINDSOR, NY 12553

TAX MAP REFERENCE: SECTION 4, BLOCK 3, LOT 11 (T. CORNWALL)  
SECTION 69, BLOCK 4 LOT 3 (T. NEW WINDSOR)

DEED REFERENCE: LIBER 1229 OF DEEDS, PAGE 781

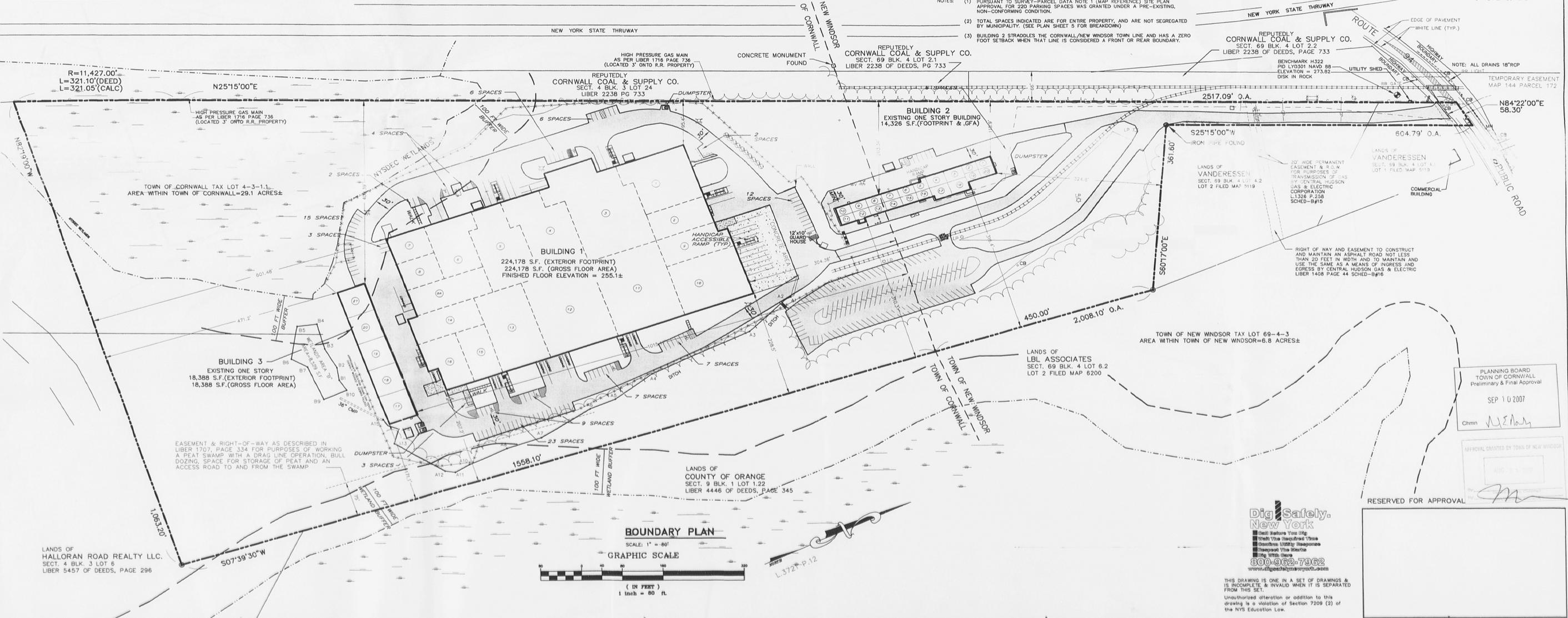
PARCEL AREA: 28.1 ACRES (T. CORNWALL)  
8.8 ACRES (T. NEW WINDSOR)

EXISTING ZONING: PIO - PLANNED INDUSTRIAL OFFICE (T. CORNWALL)  
C - DESIGN SHOPPING (T. NEW WINDSOR)

PROPOSED USES: MINI-WAREHOUSE  
GENERAL MANUFACTURING  
INDUSTRIAL PROCESSING C USE

ZONING REQUIREMENTS:

TOWN OF CORNWALL PIO ZONE	TOWN OF NEW WINDSOR D ZONE
LOT AREA: 80,000 S.F.	40,000 S.F.
LOT WIDTH: 400 FEET	200 FEET
MINIMUM ROAD FRONTAGE: 100 FEET	N/A
FRONT YARD: 100 FEET	66 FEET
REAR YARD: 50 FEET	30 FEET
TOTAL SIDE YARDS: 100 FEET	70 FEET
REAR YARD: 50 FEET	30 FEET
DEVELOPMENT COVERAGE: 70%	85%
MAXIMUM BUILDING COVERAGE: 35%	N/A
FLOOR RATIO AREA: 0.75	0.5
MAXIMUM HEIGHT: 40 FEET	12" PER FOOT OF DISTANCE TO THE NEAREST LINE



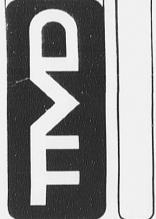
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DWG	DATE	DESCRIPTION	APPROVED
8	4/18/07	ADD DRAWING ES3 AND ES4	
6	5/19/07	ADD DRAWING R.F.T. - 1 - POND 2 LOCATION	
7	2/29/07	PARKING COMPARISON TABLE	
6	1/29/07	REVIEW COMMENTS AND PUBLIC HEARING	
5	12/19/06	PARKING CALC. BY PERCENT OF USE	
4	12/04/06	RENUMBER UNITS IN BLDG 1 AND 3	
		DESCRIPTION	
		LINE	DATE
		REVISIONS	

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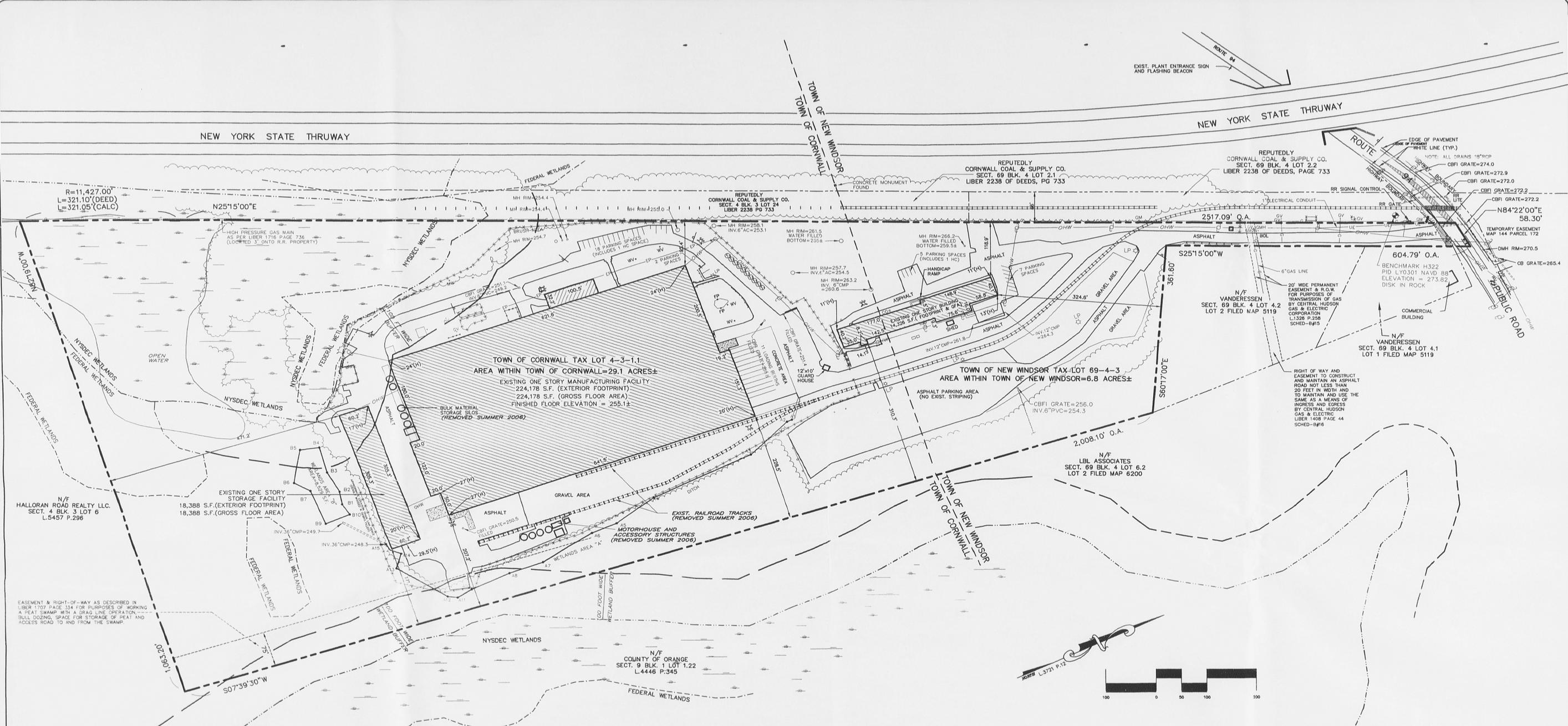


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**BOUNDARY PLAN AND ZONING DATA**

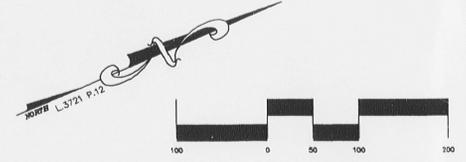
JOB: 06013  
DATE: SEPT 25, 2006  
SCALE: 1" = 80'  
DWG: SP1  
OF 10



**EXISTING CONDITIONS PLAN**  
SCALE: 1" = 80'

- EXISTING CONDITIONS SHOWN HEREON WERE OBTAINED FROM A SURVEY MAP TITLED "SURVEY OF PROPERTY KESSLER PROPERTIES SECTION 69 BLOCK 4 LOT 3 (NEW WINDSOR) SECTION 4 BLOCK 3 LOT 1.1 (CORNWALL) COUNTY OF ORANGE, NEW YORK" PREPARED BY HERITAGE LAND SURVEYING, P.C. DATED JULY 31, 2008. STRUCTURES SHOWN HEREON THAT HAVE BEEN REMOVED SINCE THE DATE OF THE SURVEY, HAVE BEEN LABELED "REMOVED SUMMER 2008".
- INDUSTRIAL PROCESS MACHINERY INSIDE OUTSIDE THE BUILDINGS ARE BEING REMOVED AND SALVAGED FROM THE PROPERTY.
- EXISTING FEDERAL AND NYSDEC WETLANDS SHOWN ARE BASED UPON ORANGE COUNTY WATER AUTHORITY MAPPING AND DATA CD'S, AND ARE AS SHOWN ON "SURVEY OF PROPERTY CITED ABOVE. ACTUAL LOCATION OF WETLAND BOUNDARY MAY VARY FROM THOSE BOUNDARIES SHOWN PENDING FIELD DELINEATION.
- WETLANDS LABELED "A" AND "B" WERE DELINEATED BY MICHAEL NOWICKI OF ECOLOGICAL SOLUTIONS ON JULY 21, 2006.

EASEMENT & RIGHT-OF-WAY AS DESCRIBED IN LIBER 1707 PAGE 134 FOR PURPOSES OF WORKING A PEAT SWAMP WITH A GRAD LINE OPERATION. BULL DOZING SPACE FOR STORAGE OF PEAT AND ACCESS ROAD TO AND FROM THE SWAMP.



NO.	DATE	DESCRIPTION	REVISIONS
3	3/19/07	CORRECT PERIMETER FENCE	
2	2/20/07	ADD REPUTED HIGHWAY BOUNDARY	
1	1/23/07	REVIEW COMMENTS AND PUBLIC HEARING	

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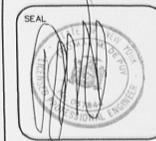
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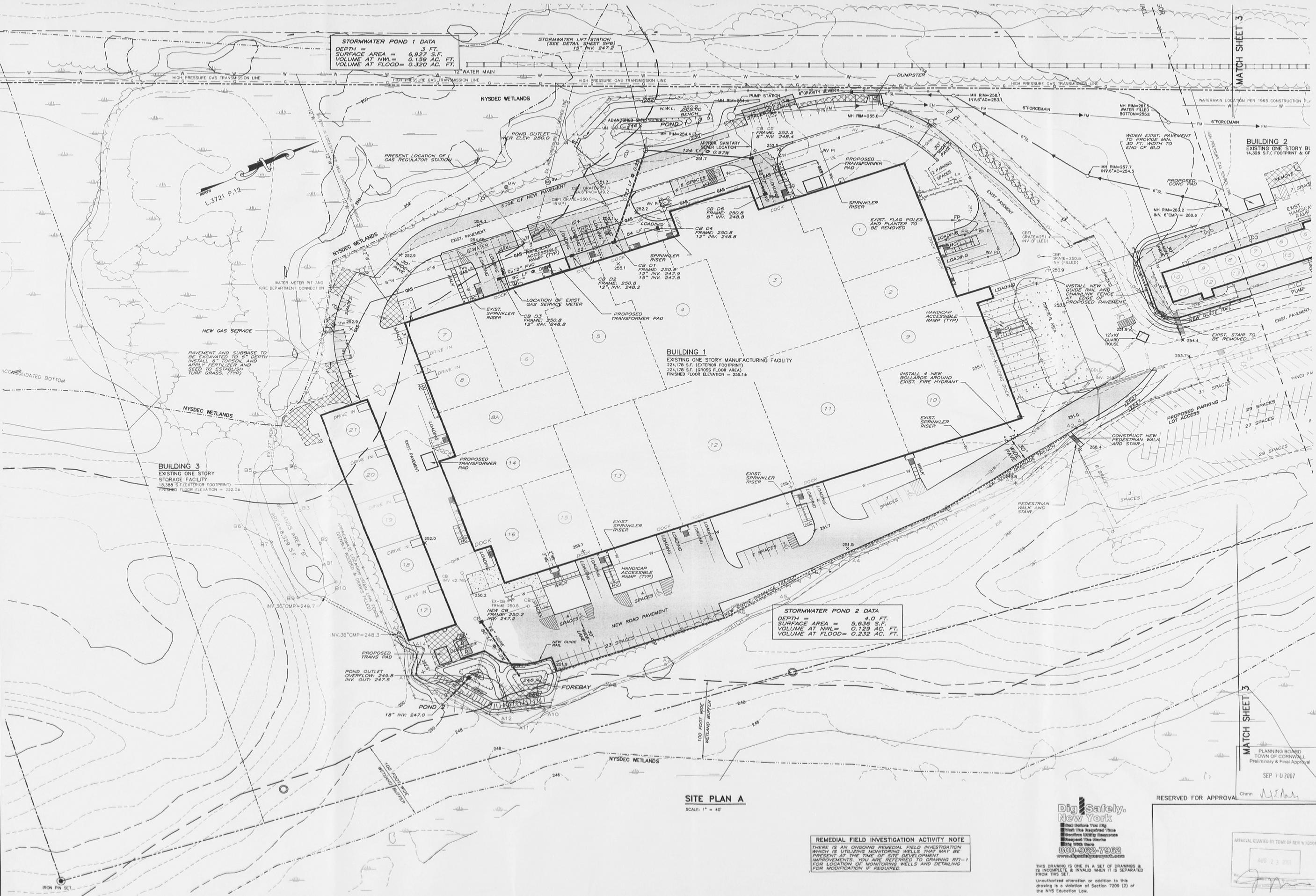
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TITLE  
**EXISTING CONDITIONS PLAN**

JOB: 06013  
DATE: NOV. 20, 2006  
SCALE: 1" = 80'  
DWG:

**SP2**  
OF 10



**SITE PLAN A**  
 SCALE: 1" = 40'

MATCH SHEET 3  
 PLANNING BOARD  
 TOWN OF CORNWALL  
 Preliminary & Final Approval  
 SEP 10 2007  
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NO.	DATE	DESCRIPTION	REVISIONS
1	12/19/06	REVIEW COMMENTS	
2	1/23/07	REVIEW COMMENTS	
3	2/20/07	REVIEW COMMENTS	
4	3/19/07	REVIEW COMMENTS	
5	4/18/07	SWMP POND DETAILS	

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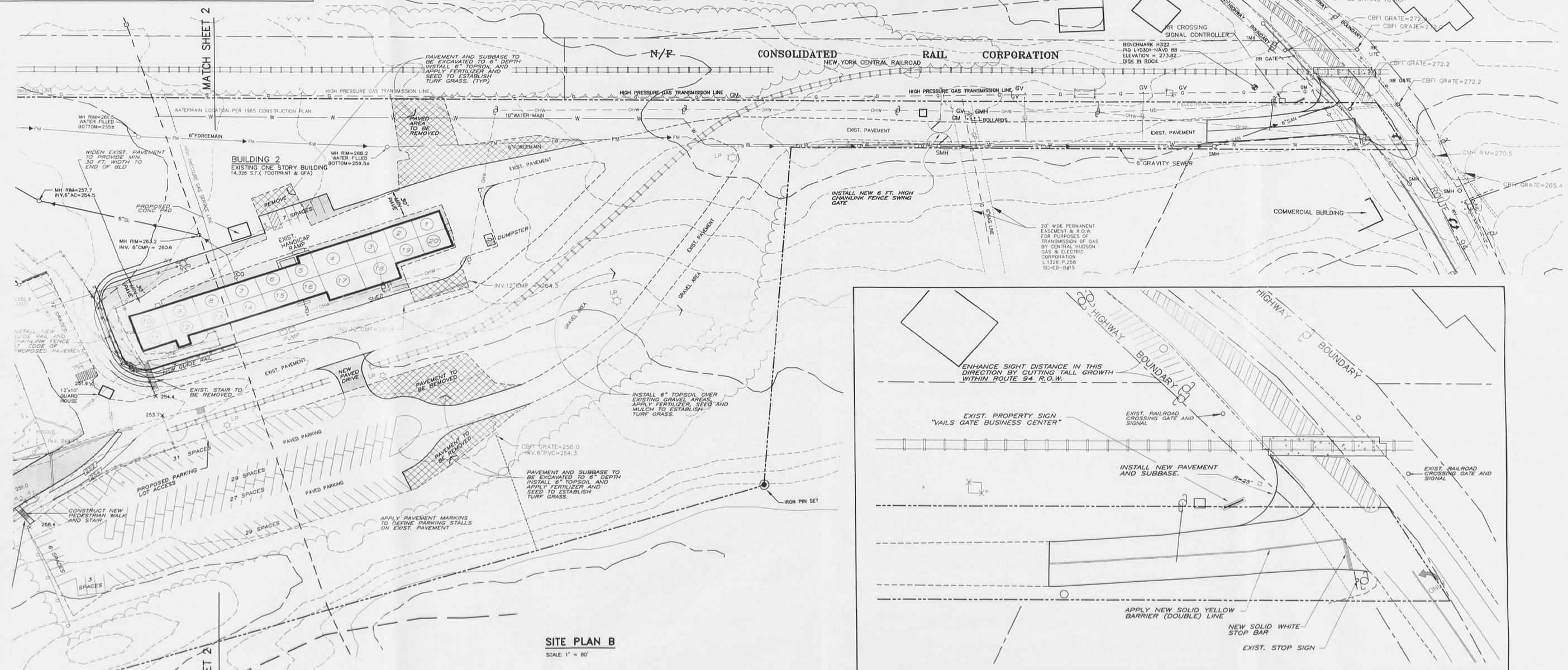


**SITE PLAN A**

JOB: 06013  
 DATE: SEPT. 25, 2006  
 SCALE: 1" = 40'  
 DWG. **SP3**  
 OF 10

**LEGEND**

SYMBOLS		ABBREVIATIONS	
	PROPERTY LINE	CB	CATCH BASIN
	ADJOINING LOT LINE	CD/P	PERF. SEPTIC CURTAIN DRAIN PIPE
	EASEMENT LINE	CL	SOLID SEPTIC CURTAIN DRAIN PIPE
	CONTOURS	CM	CORRUGATED METAL PIPE
	WATER EDGE OR STREAM	C.O.	CLEAN OUT
	GUIDE RAIL	CONC.	CONCRETE
	FENCE LINE	D.I.P.	DUCTILE IRON PIPE
	STONE WALL	ELEV.	ELEVATION
	EDGE OF PAVEMENT	FTE	FINISHED FLOOR ELEVATION
	GAS MAIN	FP	FLAG POLE (EXIST)
	OVERHEAD UTILITY LINE	GV	GAS VALVE
	TRELLIS	G.F.A.	GROSS FLOOR AREA
	ROAD SIGN	HC	HANDICAP ACCESSIBLE
	UTILITY POLE	HDPE	HIGH DENSITY POLYETHYLENE (SMOOTH INTERIOR)
	WELL	HP	HIGH POINT
	WATERMAN GATE VALE	INV	INVERT
	HYDRANT (EXIST)	LF	LINEAR FOOT
	RAILROAD TRACK	LP	LOW POINT
	DIVERSION SWALE	LSE	LOWEST SEWER ELEVATION (NV)
	AREA LIGHT POLE	MH	MANHOLE
	ROOF/FOOTING DRAINS	MW	MONITORING WELL
	WETLAND BOUNDARY LINE	N.W.L.	NORMAL WATER LEVEL (SURFACE ELEV)
	WETLAND BUFFER LINE (100 FT)	O.A.	OVER ALL (DISTANCE)
	WETLAND AREA	OE	OVERHEAD ELECTRIC
	CONCRETE SURFACES	OHW	OVERHEAD WRES
	PROPOSED BITUMINOUS SURFACES	PROP.	PROPOSED
	BITUMINOUS SURFACES (TO BE REMOVED)	ROP	REINFORCED CONCRETE PIPE
		W.L.	WATER LEVEL (SURFACE ELEV)
		WV	WATER VALVE



**GENERAL NOTES**

1. THE PLAN LOCATION OF UNDERGROUND PIPING IS BASED ON INTERPRETATION OF FEATURES VISIBLE ABOVE THE SURFACE, NO REPRESENTATION IS MADE AS TO THE ACCURACY OF UNDERGROUND PIPING SHOWN HEREON.
2. PROPOSED INTERIOR PARTITIONS SHOWN ARE TO BE MASONRY BLOCK WITH NO PASSAGE WAYS BETWEEN INDIVIDUAL UNITS. REFER TO ARCHITECTURAL DRAWINGS PREPARED BY DEGRAW AND DEHAAN ARCHITECTS, MIDDLETOWN, NEW YORK, FOR DIMENSION AND CONSTRUCTION DETAILS.

**REMEDIAL FIELD INVESTIGATION ACTIVITY NOTE**

THERE IS AN ONGOING REMEDIAL FIELD INVESTIGATION WHICH IS UTILIZING MONITORING WELLS THAT MAY BE PRESENT AT THE TIME OF SITE DEVELOPMENT IMPROVEMENTS. YOU ARE REFERRED TO DRAWING RFI-1 FOR LOCATION OF MONITORING WELLS AND DETAILING FOR MODIFICATION IF REQUIRED.

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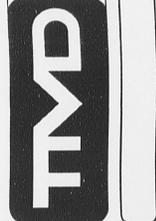
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AUG 23 2007

*[Signature]*

NO.	DATE	DESCRIPTION	REVISIONS
7	4/12/07	ADDED RFI NOTE AND INTERIM POND	
6	3/19/07	SHEET NO.	
5	2/20/07	REVIEW COMMENTS FROM PUBLIC HEARING	
4	1/23/07	REVIEW COMMENTS	
3	12/19/06	REVIEW COMMENTS	
2	11/29/06	SHEET NUMBER 3 TO 4	
1			

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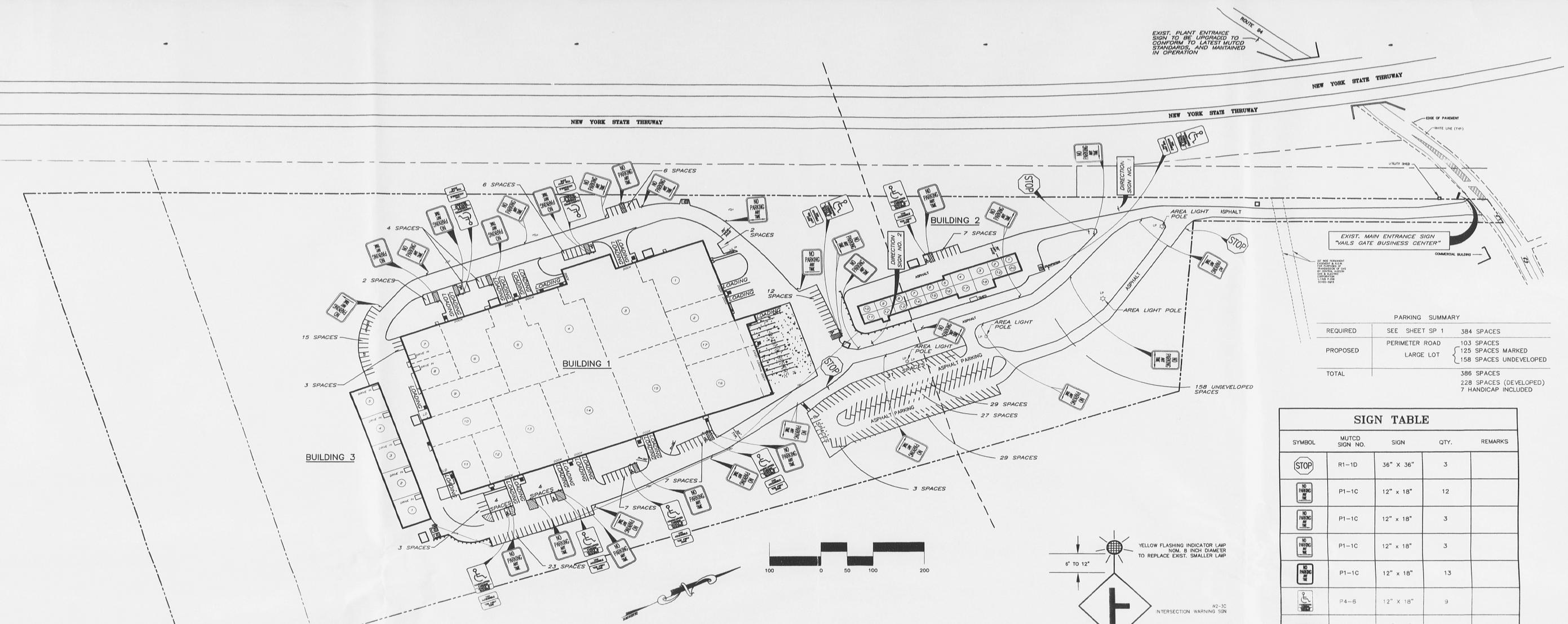


**TITLE**  
**SITE PLAN B**

**JOB** 06013  
**DATE** SEPT. 25, 2006  
**SCALE** 1" = 40'  
**DWG.**

**SP4**

OF 10



**PARKING SUMMARY**

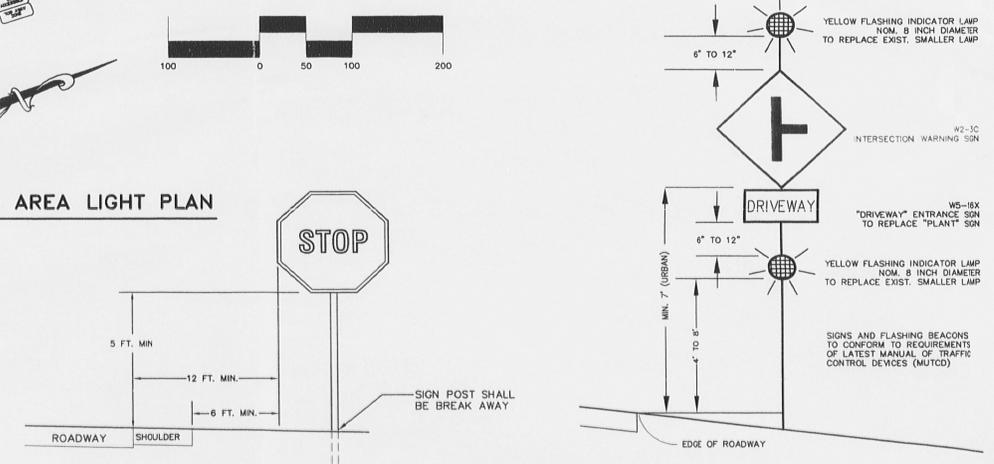
REQUIRED	SEE SHEET SP 1	384 SPACES
PROPOSED	PERIMETER ROAD	103 SPACES
	LARGE LOT	125 SPACES MARKED 158 SPACES UNDEVELOPED
TOTAL		386 SPACES
		228 SPACES (DEVELOPED)
		7 HANDICAP INCLUDED

**SIGN TABLE**

SYMBOL	MUTCD SIGN NO.	SIGN	QTY.	REMARKS
	R1-1D	36" x 36"	3	
	P1-1C	12" x 18"	12	
	P1-1C	12" x 18"	3	
	P1-1C	12" x 18"	3	
	P1-1C	12" x 18"	13	
	P4-6	12" x 18"	9	
	P3-8	12" x 6"	9	
	P3-8	12" x 6"	9	

**PAVEMENT MARKS, SIGN AND AREA LIGHT PLAN**

SCALE: 1" = 80'

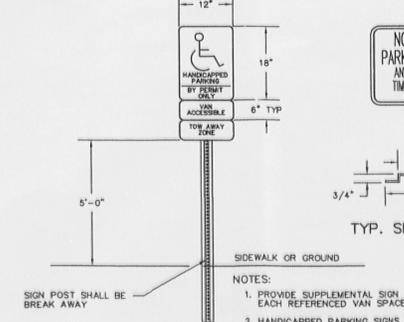


**TYPICAL STREET SIGN LOCATIONS**

N.T.S.

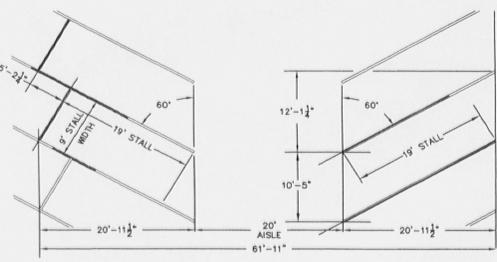
**PLANT ENTRANCE SIGN**

N.T.S.



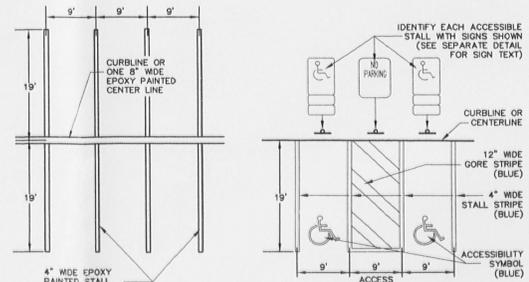
**HANDICAP PARKING SIGN DETAIL**

NOT TO SCALE



**60 DEGREE PARKING LAYOUT**

N.T.S.



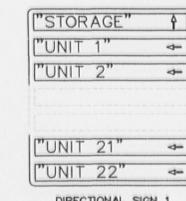
**PASSENGER CAR STALL**

**ACCESSIBLE STALL**

- NOTES:
- ALL PAVEMENT MARKINGS SHALL CONFORM TO THE STANDARDS SET FORTH IN THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD)
  - LINE AND LINE SEGMENTS USED TO INDICATE PARKING STALLS SHALL BE WHITE EPOXY PAINT. LINES USED TO INDICATE HANDICAP ACCESSIBLE PARKING STALLS AND ACCESS AISLES (GORE) SHALL BE BLUE.

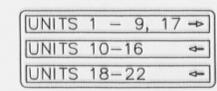
**TYPICAL PARKING STALL MARKING**

NOT TO SCALE



**DIRECTIONAL SIGN DETAILS**

NOT TO SCALE



**DIRECTIONAL SIGN 2**

NOTE: MINIMUM TEXT HEIGHT IS 3" TALL

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SEP 10 2007

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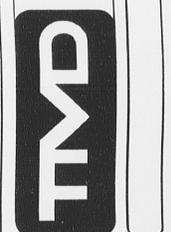
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REVISIONS

NO.	DATE	DESCRIPTION
1	12/17/06	SITE PARKING BY PERCENT USE
2	12/17/06	ADDED NO PARKING SIGNS
3	1/23/07	REVIEW COMMENTS AND PUBLIC HEARING
4	2/20/07	REV. PARALLEL MARK DETAIL
5	3/19/07	SHEET NO. PARKING LAYOUT (POND 2)
6	4/18/07	GUIDE RAILS

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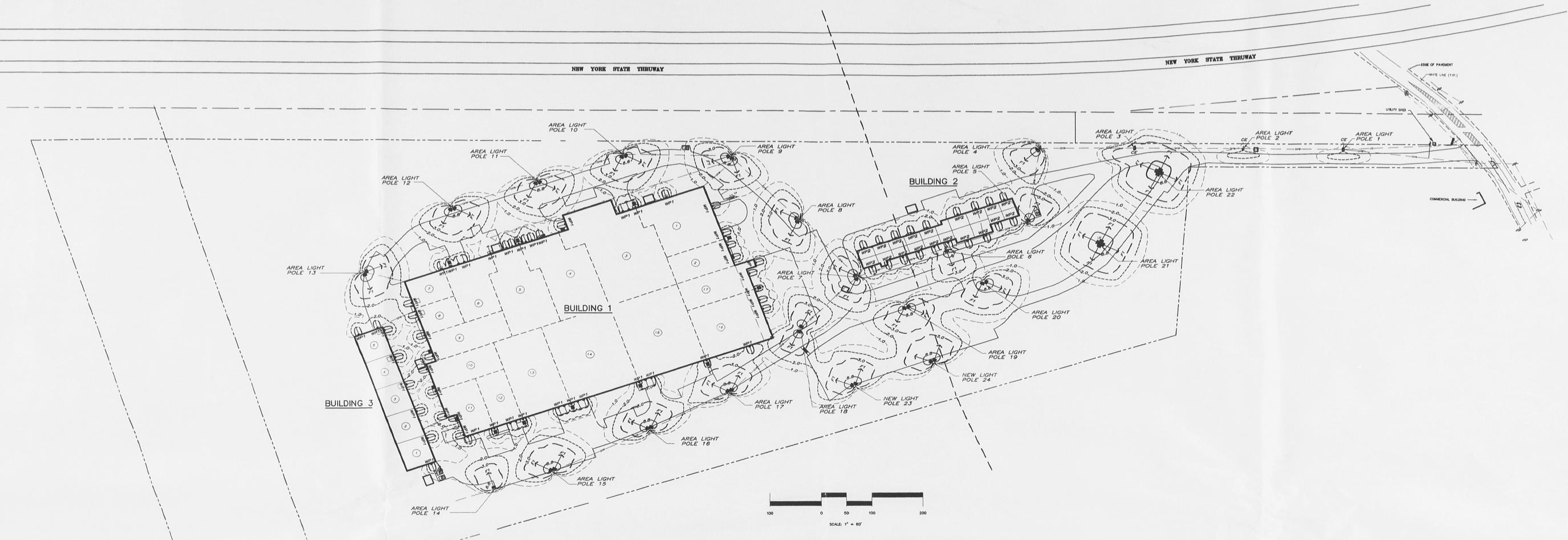


**PARKING MARKING AND SIGN PLAN**

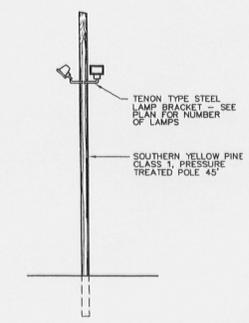
JOB 06013  
DATE NOV. 20, 2006  
SCALE AS NOTED  
DWG.

**SP5**

OF 10



SITE LIGHTING (ISOLUX)



TYPICAL LIGHT POLE  
N.T.S.

LIGHTING SCHEDULE

SYMBOL	QTY.	POLE	MODEL NO.	NOTES
WP1	20	BUILDING WALL MOUNT	SPAULDING "TULSA" LUMINAIRE SERIES TL-400HPS 400 WATT HIGH PRESSURE SODIUM	20' MOUNTING HEIGHT
WP2	8	BUILDING WALL MOUNT	SPAULDING "TULSA" LUMINAIRE SERIES TL-250HPS 250 WATT HIGH PRESSURE SODIUM	12' MOUNTING HEIGHT
⊕	4	EXIST. POLE (#4,5,6,14)	SPAULDING "BURLINGTON II" LUMINAIRE SERIES BNII-400HPS, 400 WATT HIGH PRESSURE SODIUM	SINGLE LUMINAIRE POLE 30' MOUNTING HGT. 60° AIMING ANGLE (TYP.)
⊕	2	NEW POLE (#23 & 24)		DOUBLE LUMINAIRE POLE 90° ORIENTATION 30' MOUNTING HGT.
⊕	1	EX POLE (#18)		DOUBLE LUMINAIRE POLE 180° ORIENTATION 30' MOUNTING HGT.
⊕	12	EXIST. POLE (#7-13,15-17,19,20)		DOUBLE LUMINAIRE POLE 90° ORIENTATION 30' MOUNTING HGT.
⊕	2	EXIST. POLE (#21 & 22)		QUADRUPLE LUMINAIRE POLE 360° ORIENTATION 30' MOUNTING HEIGHT
CE	3	EXIST. POLE (#1,2, & 3)	EXIST. COBRA STYLE STREET LUMINAIRE (PRESUMED GE MODEL M400-A2) HIGH PRESSURE SODIUM	SINGLE LUMINAIRE POLE 30' MOUNTING HGT.

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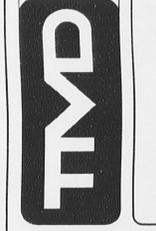
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NO.	DATE	DESCRIPTION	REVISIONS
5	3/19/07	SHEET NO. PAVEMENT CHANGE (POND 2)	
4	2/20/07	ADD LIGHT POLES 23 AND 24	
3	12/19/06	CHANGED SHEET 6 TO LIGHTING PLAN	
2	11/20/06	SHEET NUMBER 4 TO 5	
1	10/17/06	ADD FIRE PROTECTION DWG TO SET	

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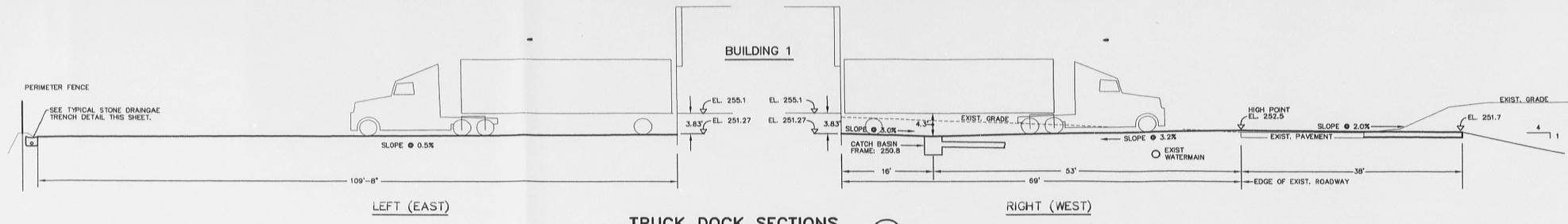
VAILS GATE BUSINESS CENTER  
KESSLER PROPERTIES  
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TOWN OF NEW WINDSOR AND TOWN OF CORNWALL  
ORANGE COUNTY, NEW YORK



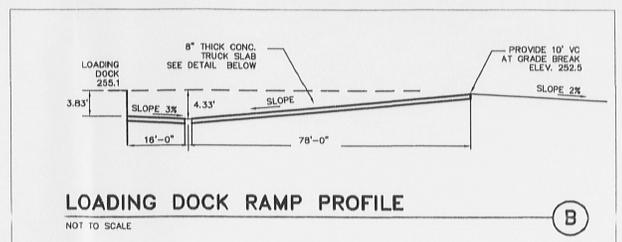
TITLE  
**SITE LIGHTING PLAN & DETAILS**

JOB 06013  
DATE SEPT 25, 2006  
SCALE AS NOTED  
DWG.

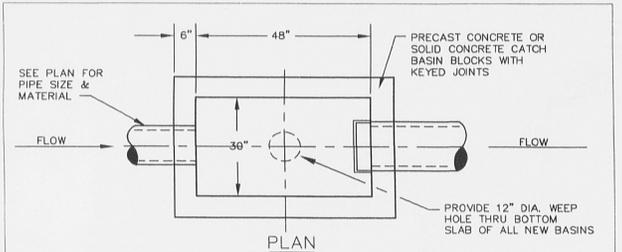
**SP6**  
OF 10



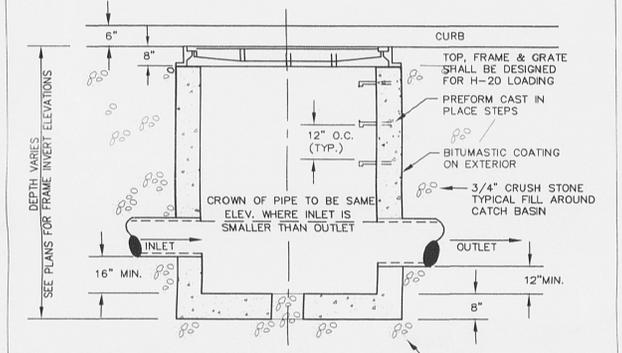
**TRUCK DOCK SECTIONS**  
SCALE: 1" = 10'



**LOADING DOCK RAMP PROFILE**  
NOT TO SCALE

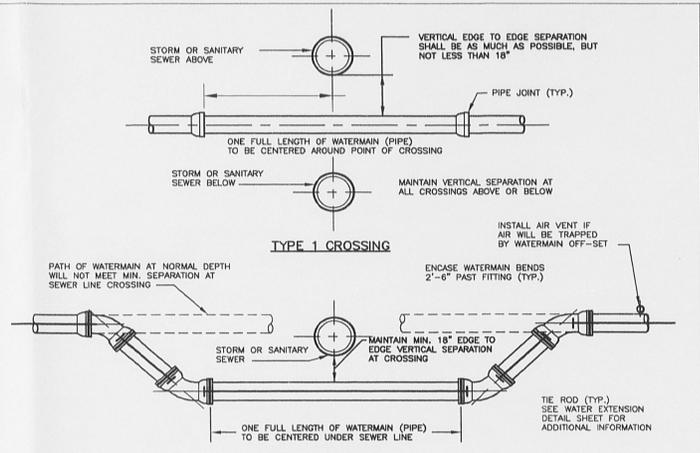


**PLAN**

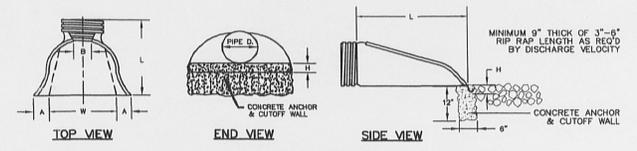


**SECTION**

**TYPICAL CATCH BASIN DETAIL**  
NOT TO SCALE



**WATERMAIN CROSSING SEWER LINE DETAILS**  
NOT TO SCALE

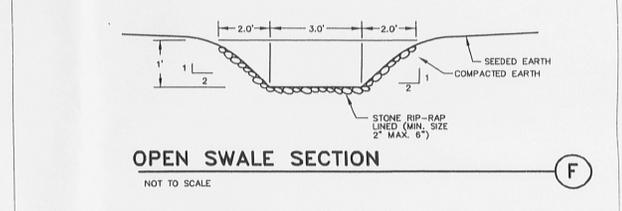


**DIMENSIONS, INCHES(MM)**

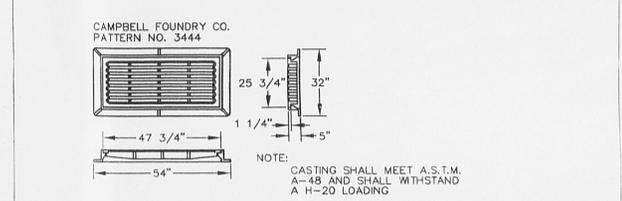
Part No.	A. +/- (125)	B MAX	H. +/- (125)	L +/- (120)	W +/- (200)
12" (300)	1210 NP	8.5(185)	10(254)	6.5(165)	25(635)
18" (450)	1810NP	7.5(190)	15(380)	6.5(165)	32(812)
24" (600)	2410 NP	7.5(190)	18(450)	6.5(165)	35(890)
30" (750)	3010 NP	10.5(266)	NA	7.0(178)	53(1346)
36" (900)	3610 NP	10.5(266)	NA	7.0(178)	53(1346)

- INSTALLATION INSTRUCTIONS**
1. SPREAD THE END SECTION COLLAR AND PLACE IT OVER THE LAST PIPE CORRUGATION. MAKE SURE THE COLLAR SEATS PROPERLY IN THE CORRUGATION VALLEY.
  2. INSERT THREADED ROD THROUGH THE PRE-DRILLED HOLES IN THE END SECTION COLLAR. TIGHTEN WING NUTS.
  3. PLACE BACKFILL AROUND THE END SECTION AND OVER THE TOP PLATE. USE CARE DURING COMPACTION ALONG THE SIDES TO AVOID DISTORTION.

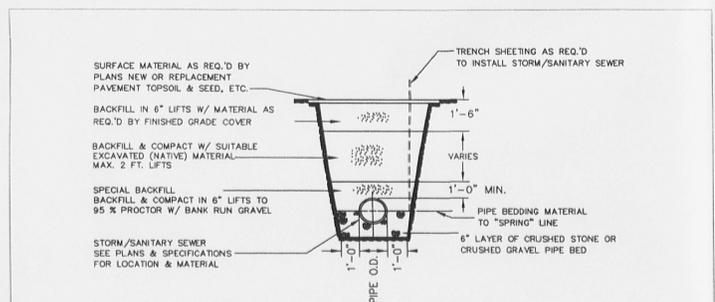
**FLARED END SECTION DETAIL**  
N.T.S.



**OPEN SWALE SECTION**  
NOT TO SCALE

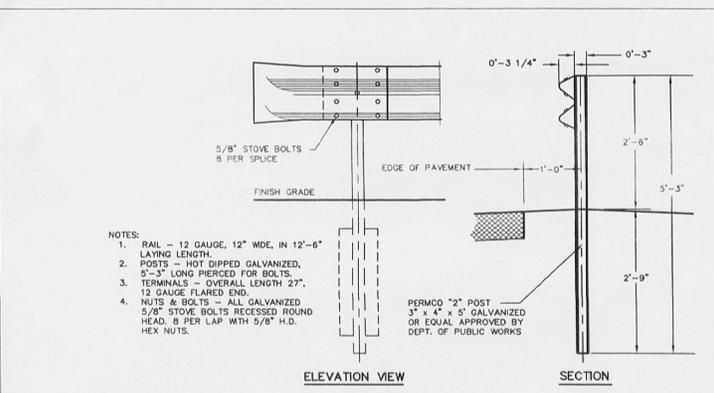


**CATCH BASIN FRAME & GRATE DETAIL**  
NOT TO SCALE

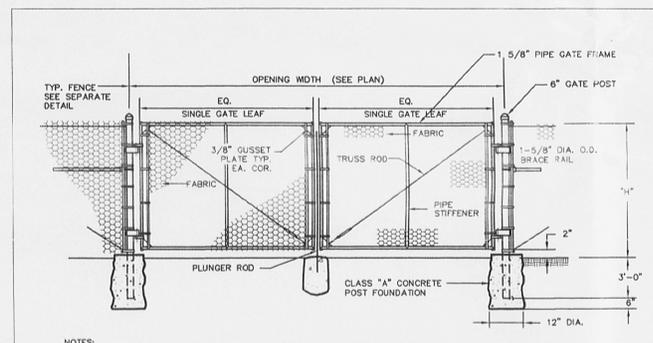


**EARTH EXCAVATION**

**TYPICAL STORM/SANITARY SEWER TRENCH DETAIL**  
NOT TO SCALE

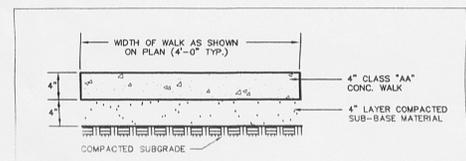


**GUIDE RAIL DETAILS**  
NOT TO SCALE

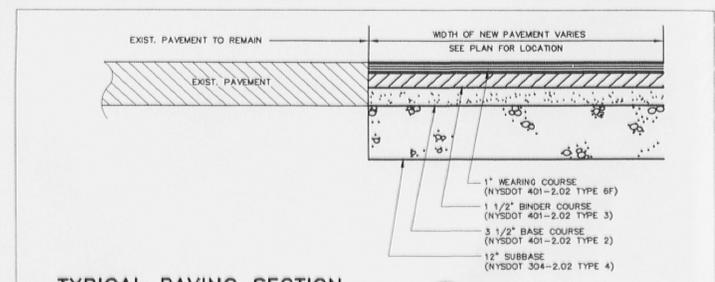


- NOTES:**
1. LETTER "H" DENOTES FABRIC WIDTH AND NOMINAL FENCE HEIGHT WHICH SHALL BE EIGHT (8 FT) FOOT UNLESS NOTED OTHERWISE.
  2. FENCE POSTS, FABRIC, FITTINGS, HARDWARE AND ACCESSORIES SHALL BE HOT DIP GALVANIZED UNLESS NOTED OTHERWISE.

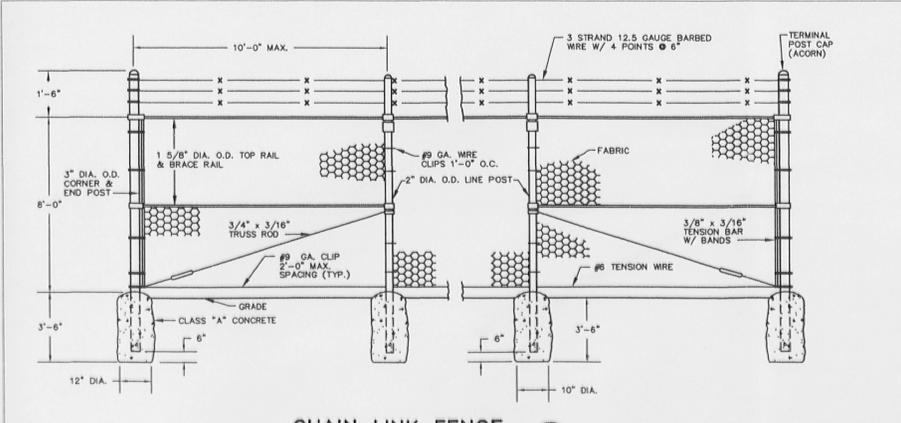
**DOUBLE SWING DRIVE GATE**  
SCALE: 1/4" = 1"



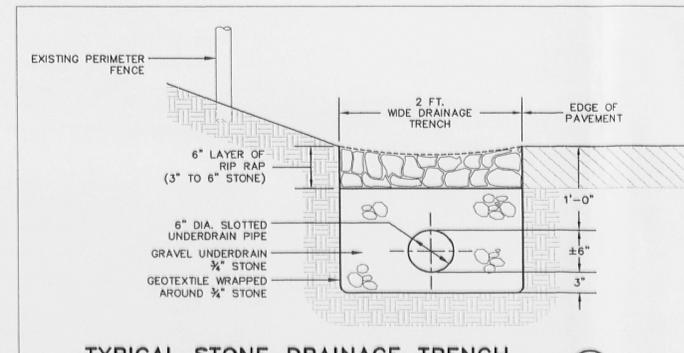
**4" CONCRETE WALK**  
NOT TO SCALE



**TYPICAL PAVING SECTION**  
NOT TO SCALE



**CHAIN LINK FENCE**  
NOT TO SCALE



**TYPICAL STONE DRAINAGE TRENCH**  
SCALE: N.T.S.

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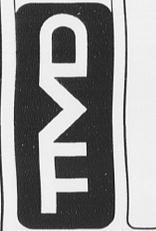
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**RESERVED FOR APPROVAL**  
TOWN OF CORNWALL  
Preliminary & Final Approval  
SEP 10 2007  
Chmn. [Signature]  
APPROVAL GRANTED BY TOWN OF NEW WINDSOR  
AUG 23 2007  
[Signature]

**ADDED AND UPDATED DETAILS**

NO.	DATE	DESCRIPTION
1	4/18/07	ADDED AND UPDATED DETAILS
2	11/20/08	REVISIONS
3	1/23/07	REVIEW COMMENTS AND PUBLIC HEARING
4	1/23/07	REVIEW COMMENTS AND PUBLIC HEARING
5	3/19/07	REVIEW COMMENTS
6	3/19/07	REVIEW COMMENTS

**T.M. DEPUY**  
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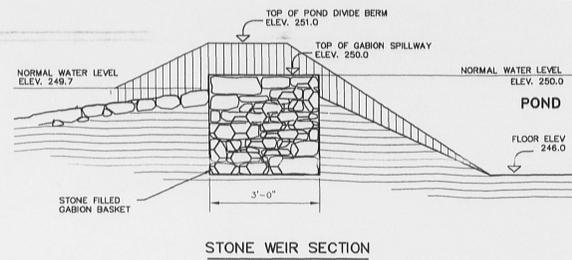
**VALLS GATE BUSINESS CENTER**  
KESSLER PROPERTIES  
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ORANGE COUNTY, NEW YORK



**SITE & UTILITY DETAILS**

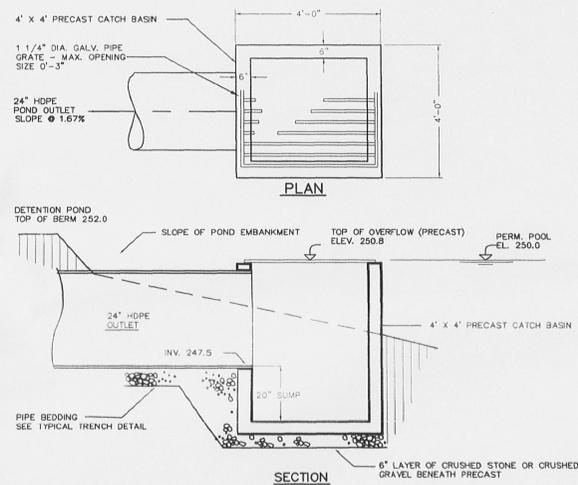
**JOB** 06013  
**DATE** SEPT. 25, 2006  
**SCALE** AS NOTED  
**DWG.**

**SP7**  
OF 10



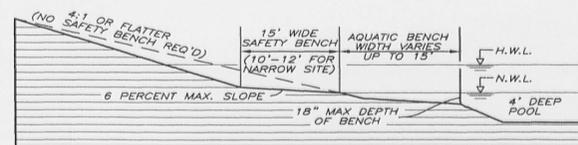
**POND 1 OUTLET STRUCTURE**

SCALE: 1/2" = 1'



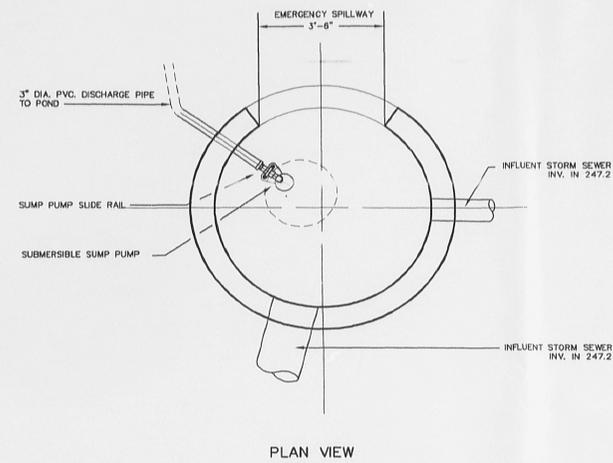
**POND 2 OUTLET STRUCTURE**

SCALE: 1/2" = 1'

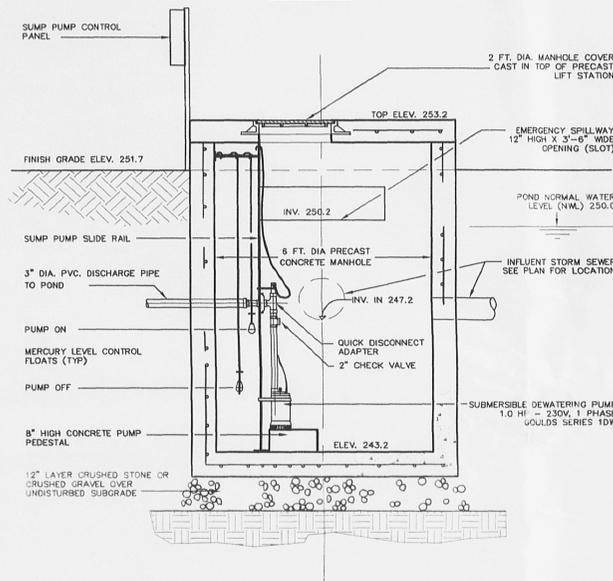


**TYPICAL POND SECTION**

SCALE: NONE



**PLAN VIEW**



**CROSS SECTION**

**STORM DRAINAGE LIFT STATION**

SCALE: 1/2" = 1'-0"

NO.	DATE	DESCRIPTION	APPROVED
5	4/18/07	UPDATE POND STRUCTURES	
4	3/19/07	SHEET NO.	
3	2/20/07	REVIEW COMMENTS	
2	11/20/06	SHEET NUMBER 6 TO 8	
1	10/17/06	30 DAY PROTECTION PING TO SET	

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VAILS GATE BUSINESS CENTER  
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ORANGE COUNTY, NEW YORK



PLANNING BOARD  
TOWN OF CORNWALL  
Preliminary & Final Approval  
SEP 10 2007  
GWHM [Signature]

APPROVAL GRANTED BY TOWN OF NEW WINDSOR  
AUG 23 2007  
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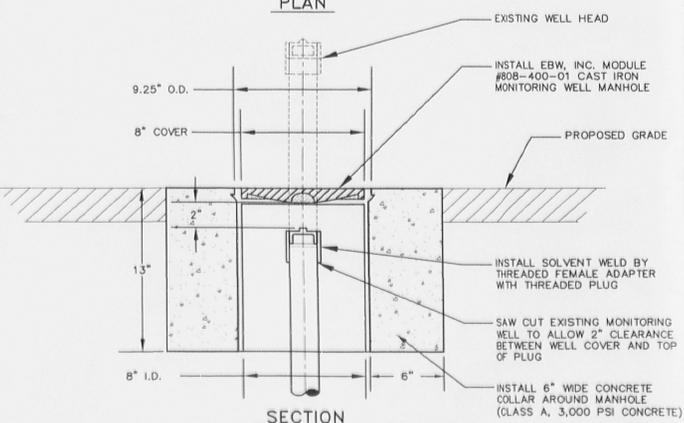
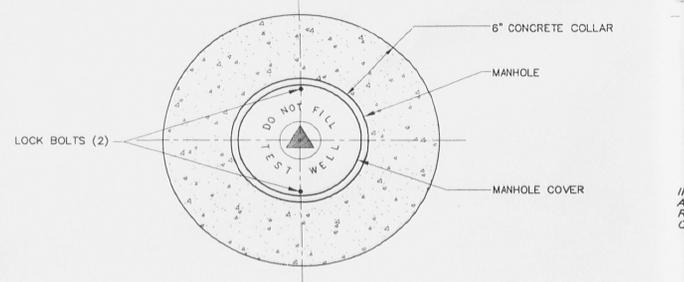
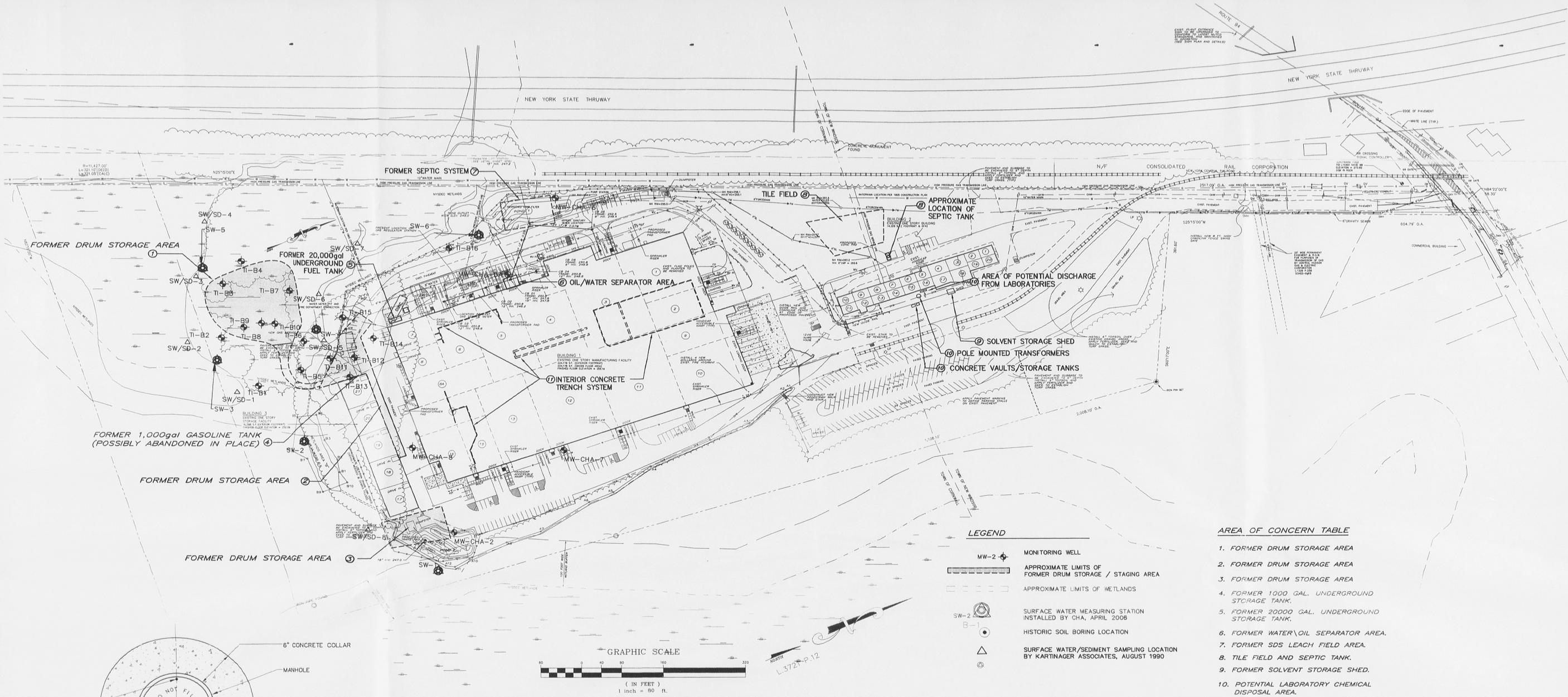
TITLE  
**STORMWATER LIFT STATION AND POND OUTLETS**

JOB 06013  
DATE SEPT. 25 2006  
SCALE AS NOTED  
DWG.

**SP8**

OF 10





**MONITORING WELL SITE IMPROVEMENT NOTE:**  
IF SITE IMPROVEMENTS BEGIN PRIOR TO THE COMPLETION OF THE RFI AND THE ASSOCIATED TESTING, THE EXISTING MONITORING WELLS SHALL BE MODIFIED WITH A FLUSH MOUNTED MANHOLE AS PER THE DETAIL INDICATED ON THIS DRAWING. IF NEW MONITORING WELLS ARE REQUIRED, THEY SHALL BE INSTALLED IN ACCORDANCE WITH THIS DETAIL.

- AREA OF CONCERN TABLE**
- FORMER DRUM STORAGE AREA
  - FORMER DRUM STORAGE AREA
  - FORMER DRUM STORAGE AREA
  - FORMER 1000 GAL. UNDERGROUND STORAGE TANK.
  - FORMER 20000 GAL. UNDERGROUND STORAGE TANK.
  - FORMER WATER/OIL SEPARATOR AREA.
  - FORMER SDS LEACH FIELD AREA.
  - TILE FIELD AND SEPTIC TANK.
  - FORMER SOLVENT STORAGE SHED.
  - POTENTIAL LABORATORY CHEMICAL DISPOSAL AREA.
  - FORMER INTERIOR TRENCH SYSTEM.

**PARCEL / OWNER DATA**

RECORD OWNER: VAILS GATE BUSINESS CENTER, LLC  
1215 ROUTE 94  
NEW WINDSOR, NY 12553

TAX MAP REFERENCE: SECTION 4, BLOCK 3, LOT 1.1 (T. CORNWALL)  
SECTION 69, BLOCK 4 LOT 3, (T. NEW WINDSOR)

DEED REFERENCE: LIBER 12229 OF DEEDS, PAGE 781

PARCEL AREA: 29.1 ACRES (T. CORNWALL)  
6.8 ACRES (T. NEW WINDSOR)

EXISTING ZONING: PIO - PLANNED INDUSTRIAL OFFICE (T. CORNWALL)  
C - DESIGN SHOPPING (T. NEW WINDSOR)

PROPOSED USES: MINI-WAREHOUSE  
GENERAL MANUFACTURING  
INDUSTRIAL PROCESSING C USE

ZONING REQUIREMENTS:	TOWN OF CORNWALL PIO ZONE	TOWN OF NEW WINDSOR C ZONE
LOT AREA:	80,000 S.F.	40,000 S.F.
LOT WIDTH:	400 FEET	200 FEET
MINIMUM ROAD FRONTAGE:	400 FEET	N/A
FRONT YARD:	100 FEET	60 FEET
SIDE YARD:	50 FEET	30 FEET
TOTAL SIDE YARDS:	100 FEET	70 FEET
REAR YARD:	50 FEET	30 FEET
DEVELOPMENT COVERAGE:	70%	85%
MAXIMUM BUILDING COVERAGE:	35%	N/A
FLOOR RATIO AREA:	0.70	0.5
MAXIMUM HEIGHT:	40 FEET	12' PER FOOT OF DISTANCE TO THE NEAREST LINE

**MONITORING WELL MODIFICATION FOR LOCATION IN TRAVEL WAY**  
SCALE: N.T.S.

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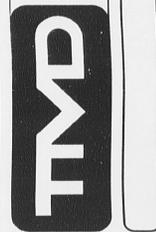
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PLANNING BOARD  
TOWN OF CORNWALL  
Preliminary & Final Approval  
SEP 10 2007  
Chmn. *[Signature]*  
APPROVAL GRANTED BY TOWN OF NEW WINDSOR  
AUG 23 2007  
*[Signature]*

NO.	DATE	DESCRIPTION	BY
2	04/12/07	ADDED DETAIL AND NOTE	MAP
1	3/28/07	POND 2 LOCATION	D.W.
1	3/19/07	PAVEMENT EDGE (POND 2)	D.W.
1	NC	DATE	BT

**TMD DEPUY**  
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2856 ROUTE 302, MIDDLETOWN, NY 10941  
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VAILS GATE BUSINESS CENTER  
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ORANGE COUNTY, NEW YORK



**RFI OVERLAY PLAN**

JOB: 06013  
DATE: MARCH 15, 2007  
SCALE: 1" = 80'  
DWG.

**RFI 1**  
OF 10

# STORMWATER POLLUTION PREVENTION PLAN (SWPPP)

## I. FOR ALL CONSTRUCTION ACTIVITIES SUBJECT TO THIS GENERAL PERMIT (GP-02-01)

- A. GENERAL**
- THE CONTRACTOR SHALL TAKE NOTICE THAT THE WATERS OF THE UNITED STATES ARE PROTECTED UNDER THE FEDERAL LAW KNOWN AS THE CLEAN WATER ACT (CWA). DISCHARGES ASSOCIATED WITH INDUSTRIAL (CONSTRUCTION) ACTIVITY TO THE WATERS OF THE UNITED STATES ARE UNLAWFUL, UNLESS AUTHORIZED BY AN ENVIRONMENTAL POLLUTANT DISCHARGE ELIMINATION SYSTEM ("NPDES") PERMIT. THE NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION ("NYSDEC") ADMINISTERS THE STATE POLLUTANT DISCHARGE ELIMINATION SYSTEM ("SPDES") PROGRAM TO COMPLY WITH THE FEDERAL STORM WATER (NPDES) REGULATIONS.
  - THE CONTRACTOR SHALL NOTE THAT THE DEVELOPER HAS FILED OR IS ABOUT TO FILE A NOTICE OF INTENT ("NOI") WITH THE NYSDEC, AND IS THE PERMITTEE REFERRED TO HEREINAFTER.
  - THE CONTRACTOR AND HIS/HER SUBCONTRACTORS ARE SUBJECT TO THE TERMS AND CONDITIONS OF THE SPDES GENERAL PERMIT ISSUED TO THE DEVELOPER FOR THIS CONSTRUCTION PROJECT. WITHOUT LIMITING THE FOREGOING, SPECIFICATION SECTION 02270 EROSION CONTROL AND SEDIMENT CONTROL PLANS PROVIDE THE DETAILED INFORMATION FOR COMPLIANCE.
- B. PROJECT DESCRIPTION**
- THE PROPERTY HAD PREVIOUSLY OPERATED AS A MANUFACTURING AND DISTRIBUTION FACILITY. THIS PROJECT PLANS TO REDEVELOPE THE SITE INTO MULTIPLE RENTAL SPACES FOR SEPARATE USES THAT ARE COMPATIBLE WITH EXISTING ZONING, WITH THE FOCUS ON PROVIDING LARGE STORAGE SPACES.
  - TO ACCOMMODATE THE ANTICIPATED VEHICLE MOVEMENTS INTO AND AROUND THE PROPERTY, EXISTING GRAVEL AREAS ARE BEING REPLACED WITH ASPHALT PAVING, AND NEW LOADING DOCK DRIVEWAYS ARE BEING ADDED.
  - UTILITY WORK WILL INCLUDE THE ADDITION OF NEW ELECTRIC, GAS, PHONE LINES, AS WELL AS WATER AND SANITARY SEWER LINES.
- C. SITE DESCRIPTION**
- THE PROPERTY IS LOCATED ALONG THE EAST SIDE OF THE FORMER NY CENTRAL RAILROAD, AND SOUTH OF NY ROUTE 94. THE SOUTHERN AND WESTERN EDGES OF THE PARCEL ARE LOCATED WITHIN A LARGE FORESTED WETLAND WITH NUMEROUS OPEN WATER PONDS AND CHANNELS. THE INTERIOR PORTION CONTAINS THREE LARGE BUILDINGS, A FORMER RAILROAD SIDING, AND PAVED PARKING AND ROADWAYS ASSOCIATED WITH A MANUFACTURING FACILITY.
  - NYS DEC REGULATED WETLANDS ARE PRESENT ON THE WESTERLY PORTION OF THE PROPERTY, AND ON THE ADDJONING PROPERTY TO THE WEST. THE ONE HUNDRED FOOT WETLAND BUFFER AREA FROM BOTH WETLANDS ARE IMPACTED BY THIS ACTIVITY.
  - VEGETATION SPECIES IN THE AREA OF THE WORK INCLUDE EASTERN RED CEDAR, OAK, MAPLE, BEECH, ASH, WHITE PINE, SPRUCE AND OTHERS TYPICAL OF NORTHEASTERN HARDWOOD FORESTS.
  - THE PRINCIPLE SOILS IN THE AREA ILLUSTRATED ON SHEET 42 OF THE SOIL SURVEY OF ORANGE COUNTY, ARE CLASSIFIED AS MAN MADE (CUT AND FILL), WITH MARON SOILS IN THE ADJONING UPLAND REGIONS, AND HISTIC HUMAQUEPTS (FRESHWATER MARSH) IN THE LOWLAND.
- D. RESPONSIBILITY AND IMPLEMENTATION OF THE STORMWATER POLLUTION PREVENTION PLAN.**
- THE DEVELOPER IS THE PERMITTEE PURSUANT TO THE NEW YORK STATE SPDES PERMIT COVERING THE WORK. THE CONTRACTOR AND HIS/HER SUBCONTRACTORS, IS THE OPERATOR OF ANY EROSION CONTROL PRACTICE INSTALLED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS, OR AS DIRECTED.
  - UPON COMPLETION OF WORK, AND ACCEPTANCE OF THE STORMWATER POLLUTION PREVENTION SYSTEM CONSTRUCTED IN ACCORDANCE WITH THESE PLANS, THE PROPERTY OWNER WILL BE RESPONSIBLE FOR THE OPERATION AND MAINTENANCE OF SAID SYSTEM IN ACCORDANCE WITH THE CONDITIONS OF SAID "SPDES" PERMIT. THE PROPERTY OWNER WILL BE THE OPERATOR REFERRED TO HEREINAFTER.
- E. CONSTRUCTION SEQUENCE**
- PRIOR TO COMMENCING WORK, CONTACT "DIG SAFELY NEW YORK" TO MARK UNDERGROUND UTILITIES. CENTRAL HUDSON'S HIGH PRESSURE GAS TRANSMISSION LINE IS ON THE SITE.
  - INSTALL STRAW BALE DIKES AND OTHER EROSION CONTROL FEATURES AROUND PERIMETER OF AREAS TO BE DISTURBED AS SHOWN ON THE PLANS.
  - INSTALL ROCK CONSTRUCTION SITE ENTRANCE AT POINT WHERE ACCESS TO THE WORK IS BEING OBTAINED FROM A TOWN ROAD OR HIGHWAY, (E.G., ROUTE 94).
  - INSTALL UNDERGROUND UTILITIES. PROVIDE SEDIMENT TRAPS AT EACH STORMWATER INLET.
  - INSTALL AND COMPACT ROAD SUBBASE MATERIAL, CURBS AND PAVEMENT IN ACCORDANCE WITH THE PLANS.
  - FINISH GRADE COMPLETED AREAS TO BLEND CUT/FILL AND DISTURBED GROUND INTO SURROUNDING INSTANT SURFACE, SCULPTURE, AND STABILIZE EROSION CONTROL FEATURES TO BE PERMANENT.
  - REMOVE SILT FENCE, STRAW BALE DIKES, STABILIZED CONSTRUCTION ENTRANCES AND OTHER TEMPORARY EROSION CONTROL FEATURES WHEN PERMANENT VEGETATIVE COVER HAS ESTABLISHED ON EXPOSED EARTH SURFACES, AND RISK OF SOIL EROSION IS LOW, CONSISTENT WITH THE NEW YORK GUIDELINES FOR URBAN EROSION AND SEDIMENT CONTROL. THERE SHALL NOT BE MORE THAN FIVE (5) SECTORS OF SOIL AT ANY ONE TIME, WITHOUT PRIOR WRITTEN APPROVAL FROM THE DEPARTMENT (NYSDEC).

- F. POLLUTION PREVENTION MEASURES**
- THE CONTRACTOR SHALL PROVIDE DUST CONTROL, AS REQUIRED, ON A REGULAR BASIS TO ELIMINATE THE MIGRATION OF AIRBORNE SOIL PARTICLES. DUST CONTROL IS TO BE ACHIEVED BY SPRINKLING WITH WATER.
  - THE CONTRACTOR SHALL PROVIDE LITTER CONTROL WITH THE USE OF COVERED TRASH RECEPTACLES, AND THE REGULAR CLEAN UP OF WASTE MATERIALS, BARRIERS, CONTAINERS, PACKAGING AND SIMILAR TRASH GENERATED BY THE WORK AND WORKERS.
  - ALL OILS, GREASE, SOLVENTS AND OTHER FLUIDS NECESSARY FOR THE MAINTENANCE AND OPERATION OF EQUIPMENT USED FOR THE WORK ARE TO BE HANDLED PROPERLY, CONTAINED AND COLLECTED FOR USE OR DISPOSAL AT THE APPROPRIATE RECYCLING CENTER OR COLLECTION SITE.
- G. CONSTRUCTION WASTE MATERIAL**
- DEBRIS AND TRASH FROM CONSTRUCTION ACTIVITY SHALL BE COLLECTED BY THE CONTRACTOR ON A DAILY BASIS. THIS WASTE MATERIAL IS TO BE STORED IN CLOSED CONTAINERS (SEE E (3) ABOVE) AND IMMEDIATELY REMOVED FROM THE SITE AND DISPOSED OF AT AN APPROVED DISPOSAL SITE. NO ON SITE BURIAL OF MATERIAL IS PERMITTED.
- H. STRUCTURAL AND VEGETATIVE MEASURES FOR SOIL STABILIZATION, RUNOFF CONTROL AND SEDIMENT CONTROL**
- TEMPORARY CONTROLS
    - INSTALL STABILIZED ROCK CONSTRUCTION ENTRANCE PRIOR TO CONSTRUCTION TRAFFIC ENTERING AND LEAVING SITE.
    - INSTALL SEDIMENT BARRIER (SILT) FENCE PRIOR TO ANY EXCAVATION WORK (INCLUDING STUMP REMOVAL).
    - CONSTRUCT TEMPORARY DIVERSION SWALES AT ANY POINT WHERE SURFACE WATER CAN BE DIRECTED AWAY FROM DISTURBED SURFACES. IF A SURFACE WATER ROUTE CROSSES A DISTURBED AREA, PROVIDE A RIP-RAP CHANNEL AS SOON AS POSSIBLE AFTER THE WATER ROUTE.
    - PICK UP TRASH AND LIGHTWEIGHT RUBBISH THAT MAY BE SWEEP AWAY BY WIND.
    - FILL OR GRADE VEHICLE TRACKS TO AVOID EROSION.
  - PERMANENT CONTROLS
    - SEED AND MULCH EXPOSED EARTH AREAS TO ESTABLISH PERMANENT VEGETATIVE COVER AS SOON AS PRACTICAL AFTER FINAL GRADING. MULCH SHALL FOLLOW SEEDING BY NO MORE THAN 24 HOURS.
    - STORMWATER QUALITY PONDS 1 AND 2 ARE TO CLEANSE RUNOFF FROM PROPOSED PAVEMENT IMPROVEMENTS.
  - TEMPORARY CONTROLS THAT ARE TO BE CONVERTED TO PERMANENT STORMWATER MANAGEMENT FACILITIES
    - THERE ARE NO TEMPORARY MEASURES PROPOSED THAT WILL BE PERMANENT.
  - IMPLEMENTATION SCHEDULE OF EROSION AND SEDIMENT CONTROLS
    - SEDIMENT BARRIER (SILT) FENCE SHALL BE INSTALLED PRIOR TO ANY SITE DISTURBANCE.
    - STABILIZED ROCK CONSTRUCTION ENTRANCE SHALL BE INSTALLED PRIOR TO HEAVY TRUCK TRAFFIC ENTERING OR LEAVING THE SITE.
    - TEMPORARY DIVERSION SWALES SHALL BE INSTALLED AS THE WORK PROCEEDS.
    - STRAW BALE DIKES SHALL BE PLACED WHERE NEEDED IN CONJUNCTION WITH SWALES AND STORMWATER INLETS, AS THE WORK PROCEEDS.
    - DUST CONTROL SHALL BE CONDUCTED DURING DRY WEATHER AS NEEDED OR DIRECTED.
  - MAINTENANCE SCHEDULE FOR EROSION AND SEDIMENT CONTROLS
    - SEDIMENT BARRIER (SILT) FENCE: MAINTAIN PER DETAIL.
    - STABILIZED ROCK CONSTRUCTION ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT PREVENTS TRAFFIC FROM TRACKING SEDIMENT ONTO PUBLIC ROADWAYS. INSPECT AFTER EACH RAIN AND PERIODICALLY REDRESS THE TOP AGGREGATE TO MAINTAIN EFFECTIVENESS.
    - STRAW BALE DIKES REQUIRE FREQUENT INSPECTION AFTER EACH RAIN, AND SHALL BE REMOVED AND REPLACED WHEN THEY HAVE SERVED THEIR USEFULNESS.
    - TEMPORARY DIVERSION SWALES SHALL BE INSPECTED AFTER EACH RAIN; REPAIR ANY EROSION DAMAGE AND REMOVE ANY ACCUMULATION OF SEDIMENT IN SWALE OR IN OUTLET AREA TO RESTORE OPERATION.

## III. STANDARD PERMIT CONDITIONS (DEPARTMENT: NYS DEPT. OF ENVIRONMENTAL CONSERVATION)

- DUTY TO COMPLY: THE OPERATOR MUST COMPLY WITH ALL CONDITIONS OF THIS PERMIT. ALL CONTRACTORS AND SUBCONTRACTORS ASSOCIATED WITH THE PROJECT MUST COMPLY WITH THE TERMS OF THE SWPPP. ANY PERMIT NON-COMPLIANCE CONSTITUTES A VIOLATION OF THE CLEAN WATER ACT (CWA) AND THE ENVIRONMENTAL CONSERVATION LAW, AND IS GROUNDS OF AN ENFORCEMENT ACTION AGAINST EITHER THE OPERATOR OR THE CONTRACTOR/SUBCONTRACTOR. PERMIT REVOCATION OR MODIFICATION, OR DENIAL OF A PERMIT RENEWAL APPLICATION, UPON A FINDING OF SIGNIFICANT NON-COMPLIANCE WITH THIS PERMIT OR THE APPLICABLE SWPPP. THE DEPARTMENT MAY ORDER AN IMMEDIATE STOP TO ALL CONSTRUCTION ACTIVITY AT THE SITE UNTIL THE NON-COMPLIANCE IS REMEDIATED. THE STOP WORK ORDER SHALL BE IN WRITING, SHALL DESCRIBE THE NON-COMPLIANCE IN DETAIL, AND SHALL BE SENT TO THE OPERATOR OR THE OPERATOR'S ON-SITE REPRESENTATIVE.
- CONTINUATION OF THE EXPIRED GENERAL PERMIT: THIS PERMIT EXPIRES FIVE (5) YEARS AFTER ISSUANCE ON JANUARY 8, 2008. HOWEVER, COVERAGE MAY BE OBTAINED UNDER THE EXPIRED GENERAL PERMIT WHICH WILL CONTINUE IN FORCE, AND EFFECT UNTIL A NEW GENERAL PERMIT IS ISSUED, AFTER ISSUANCE OF A NEW GENERAL PERMIT. THOSE WITH COVERAGE UNDER GP-02-01 WILL HAVE SIX (6) MONTHS FROM THE EFFECTIVE DATE OF THE NEW GENERAL PERMIT TO COMPLETE THEIR PROJECT OR OBTAIN COVERAGE UNDER THE NEW PERMIT. UNLESS OTHERWISE NOTICED BY THE DEPARTMENT, OPERATORS SEEKING AUTHORIZATION UNDER A NEW GENERAL PERMIT MUST SUBMIT A NEW NOI (NOTICE OF INTENT) IN ACCORDANCE WITH THE TERMS OF SUCH NEW GENERAL PERMIT.
- PENALTIES FOR VIOLATIONS OF PERMIT CONDITIONS: THERE ARE SUBSTANTIAL CRIMINAL, CIVIL, AND ADMINISTRATIVE PENALTIES ASSOCIATED WITH VIOLATING THE PROVISIONS OF THIS PERMIT. FINES UP TO \$25,000 PER DAY FOR EACH VIOLATION AND IMPRISONMENT FOR UP TO FIFTEEN (15) YEARS MAY BE ASSESSED DEPENDING UPON THE NATURE AND DEGREE OF THE OFFENSE. THE SUPERVISOR AND INSPECTOR REQUIRED BY THIS GENERAL PERMIT ARE PUBLIC DOCUMENTS THAT THE OPERATOR MUST MAKE AVAILABLE FOR INSPECTION, REVIEW AND APPROVAL BY THE DEPARTMENT. THE OPERATOR SHALL BE RESPONSIBLE FOR THE COSTS OF ANY INSPECTION, REVIEW AND APPROVAL BY THE DEPARTMENT RECEIVING A WRITTEN REQUEST BY ANY SUCH PERSON TO REVIEW THE SWPPP AND/OR THE INSPECTION REPORTS. COPYING OF DOCUMENTS WILL BE DONE AT THE REQUESTER'S EXPENSE.
- NEED TO HALT OR REDUCE ACTIVITY: THIS SHALL NOT BE A DEFENSE FOR A PERMITTEE IN AN ENFORCEMENT ACTION THAT IT WOULD HAVE BEEN NECESSARY TO HALT OR REDUCE THE CONSTRUCTION ACTIVITY IN ORDER TO MAINTAIN COMPLIANCE WITH THE CONDITIONS OF THIS PERMIT.
- TAKE TO MITIGATE: THE PERMITTEE AND ITS CONTRACTORS AND SUBCONTRACTORS SHALL TAKE ALL REASONABLE STEPS TO MINIMIZE OR PREVENT ANY DISCHARGE IN VIOLATION OF THIS PERMIT WHICH HAS A REASONABLE LIKELIHOOD OF ADVERSELY AFFECTING HUMAN HEALTH OR THE ENVIRONMENT.
- PROVIDE INFORMATION: THE PERMITTEE SHALL FURNISH ANY INFORMATION REQUESTED BY ANY AGENCY WITH REGULATORY OR REVIEW AUTHORITY OVER THIS PROJECT FOR THE PURPOSE OF DETERMINING COMPLIANCE WITH THIS PERMIT OR COMPLIANCE WITH ANY OTHER REGULATORY REQUIREMENTS PLACED ON THE PROJECT IN CONJUNCTION WITH THIS PERMIT. FAILURE TO PROVIDE REQUESTED INFORMATION SHALL BE A VIOLATION OF THIS PERMIT. SUCH REGULATING AGENCIES INCLUDE BUT ARE NOT LIMITED TO THE DEPARTMENT, SWSD, LOCAL PLANNING, ZONING, HEALTH, AND BUILDING DEPARTMENTS THAT REVIEW AND APPROVE EROSION AND SEDIMENT CONTROL PLANS, GRADING PLANS, AND STORM WATER MANAGEMENT PLANS. THESE AGENCIES INTO WHOSE SYSTEM RUNOFF FROM THE PERMITTED PROJECT OR ACTIVITY DISCHARGES.
- OTHER INFORMATION: WHEN THE PERMITTEE BECOMES AWARE THAT HE OR SHE FAILED TO SUBMIT ANY RELEVANT FACTS OR SUBMITTED INCORRECT INFORMATION IN THE NOI OR IN ANY OTHER REPORT TO THE DEPARTMENT, HE OR SHE SHALL PROMPTLY SUBMIT SUCH FACTS OR INFORMATION.
- SIGNATORY REQUIREMENTS: ALL NOIS, NOTIS, SWPPS, REPORTS, CERTIFICATIONS OR INFORMATION REQUIRED BY THIS PERMIT OR SUBMITTED PURSUANT TO THIS PERMIT, SHALL BE SIGNED AS FOLLOWS:
  - ALL NOIS & NOTIS SHALL BE SIGNED AS FOLLOWS:
    - FOR A CORPORATION: BY (1) A PRESIDENT, SECRETARY, TREASURER, OR VICE PRESIDENT OF THE CORPORATION IN CHARGE OF A PRINCIPLE BUSINESS FUNCTION, OR ANY OTHER PERSON AUTHORIZED TO OR WHO PERFORMS SIMILAR POLICY OR DECISION-MAKING FUNCTIONS FOR THE CORPORATION; OR (2) THE MANAGER OF ONE OR MORE MANUFACTURING, PRODUCTION, OR OPERATING FACILITIES EMPLOYING MORE THAN 250 PERSONS OR HAVING GROSS ANNUAL SALES OR EXPENDITURES EXCEEDING \$25,000,000 (IN SECOND-QUARTER 1980 DOLLARS) IF AUTHORITY TO SIGN DOCUMENTS HAS BEEN ASSIGNED OR DELEGATED TO THE MANAGER IN ACCORDANCE WITH CORPORATE PROCEDURES;
    - FOR PARTNERSHIP OR SOLE PROPRIETORSHIP: BY A GENERAL PARTNER OR THE PROPRIETOR, RESPECTIVELY; OR (4) FOR A MUNICIPALITY, STATE, FEDERAL, OR OTHER PUBLIC AGENCY, BY EITHER A PRINCIPLE EXECUTIVE OFFICER OR RANKED ELECTED OFFICIAL, FOR PURPOSES OF THIS SECTION, A PRINCIPLE EXECUTIVE OFFICER OF A FEDERAL AGENCY INCLUDES (1) THE CHIEF EXECUTIVE OFFICER OF THE AGENCY, OR (2) A SENIOR EXECUTIVE OFFICER HAVING RESPONSIBILITY FOR THE OVERALL OPERATIONS OF A PRINCIPLE GEOGRAPHIC UNIT OF THE AGENCY (E.G. REGIONAL ADMINISTRATORS OF THE EPA).
  - FOR AN INDIVIDUAL SPDES PERMIT: BY THE INDIVIDUAL SPDES PERMITTEE.

- PROPER OPERATION AND MAINTENANCE: THE PERMITTEE SHALL AT ALL TIMES PROPERLY OPERATE AND MAINTAIN ALL FACILITIES AND SYSTEMS OF TREATMENT AND CONTROL (AND RELATED APPURTENANCES) WHICH ARE INSTALLED OR USED BY THE PERMITTEE TO ACHIEVE COMPLIANCE WITH THE CONDITIONS OF THIS PERMIT AND WITH THE REQUIREMENTS OF SWPPS. PROPER OPERATION AND MAINTENANCE ALSO INCLUDES ADEQUATE LABORATORY CONTROLS APPROPRIATE QUALITY ASSURANCE PROCEDURES. PROPER OPERATION AND MAINTENANCE REQUIRES THE OPERATION OF BACKUP OR AUXILIARY FACILITIES OR SIMILAR SYSTEMS, INSTALLED BY A PERMITTEE ONLY WHEN NECESSARY TO ACHIEVE COMPLIANCE WITH THE CONDITIONS OF THE PERMIT.
- INSPECTION AND ENTRY: THE PERMITTEE SHALL ALLOW THE DEPARTMENT OR AN AUTHORIZED REPRESENTATIVE OF EPA, THE STATE, OR, IN THE CASE OF A CONSTRUCTION SITE WHICH DISCHARGES THROUGH AN MS4, AN AUTHORIZED REPRESENTATIVE OF THE MS4 RECEIVING THE DISCHARGE, UPON THE PRESENTATION OF CREDENTIALS AND OTHER DOCUMENTS AS MAY BE REQUIRED BY LAW, TO:
  - ENTER UPON THE PERMITTEE'S PREMISES WHERE A REGULATED FACILITY OR ACTIVITY IS LOCATED OR CONDUCTED OR WHERE RECORDS MUST BE KEPT UNDER THE CONDITIONS OF THIS PERMIT;
  - HAVE ACCESS TO AND COPY AT REASONABLE TIMES, ANY RECORDS THAT MUST BE KEPT UNDER THE CONDITIONS OF THIS PERMIT; AND
  - INSPECT AT REASONABLE TIMES ANY FACILITIES OR EQUIPMENT (INCLUDING MONITORING AND CONTROL EQUIPMENT).
- PERMIT ACTIONS: AT THE DEPARTMENT'S SOLE DISCRETION, THIS PERMIT MAY, AT ANY TIME, BE MODIFIED, REVOKED OR RENEWED. THE FILING OF A REQUEST BY THE PERMITTEE FOR A PERMIT MODIFICATION, REVOCATION AND REISSUANCE, TERMINATION, A NOTIFICATION OF PLANNED CHANGES OR ANTICIPATED NONCOMPLIANCE DOES NOT STAY COMPLIANCE WITH ANY TERMS OF THIS PERMIT.

## IV. RECORDS, REPORTING & RETENTION OF RECORDS

- THE DEPARTMENT MAY, AT ITS SOLE DISCRETION, REQUIRE MONITORING OF DISCHARGE(S) FROM THE PERMITTED CONSTRUCTION ACTIVITY AFTER NOTIFYING THE PERMITTEE IN WRITING OF THE BASIS FOR SUCH MONITORING AND THE ASSOCIATED REPORTING REQUIREMENTS, IF ANY.
  - THE OPERATOR SHALL RETAIN COPIES OF SWPPS AND ANY REPORTS SUBMITTED IN CONJUNCTION WITH THIS PERMIT AND RECORDS OF ALL DATA USED TO COMPLETE THE NOI TO BE COVERED BY THIS PERMIT, FOR A PERIOD OF AT LEAST THREE YEARS FROM THE DATE THAT THE SITE IS FINALLY STABILIZED. THIS PERIOD MAY BE EXTENDED BY THE DEPARTMENT, IN ITS SOLE DISCRETION, AT ANY TIME UPON WRITTEN NOTIFICATION.
  - THE OPERATOR SHALL RETAIN A COPY OF THE SWPPP REQUIRED BY THIS PERMIT AT THE CONSTRUCTION SITE FROM THE DATE OF INITIATION OF CONSTRUCTION ACTIVITIES TO THE DATE OF FINAL STABILIZATION.
  - THE OPERATOR SHALL ALSO PREPARE A WRITTEN SUMMARY OF ITS STATUS WITH RESPECT TO COMPLIANCE WITH THIS GENERAL PERMIT AT A MINIMUM FREQUENCY OF EVERY THREE MONTHS DURING WHICH COVERAGE OF THIS PERMIT EXISTS. THE SUMMARY SHOULD ADDRESS THE STATUS OF ACHIEVING EACH COMPONENT OF THE SWPPP. ADDRESSES:
 

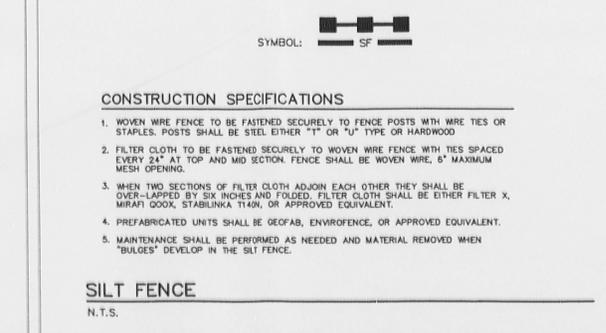
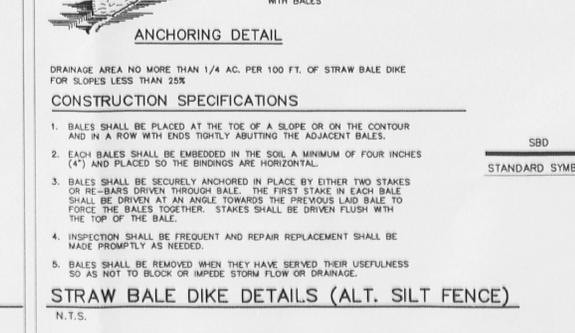
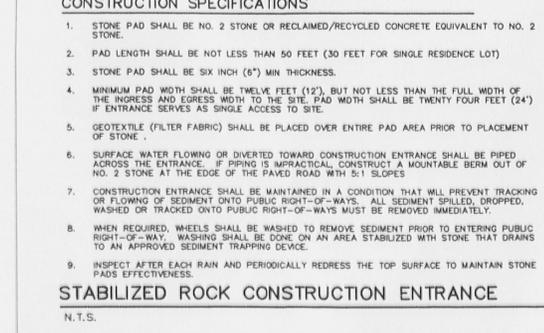
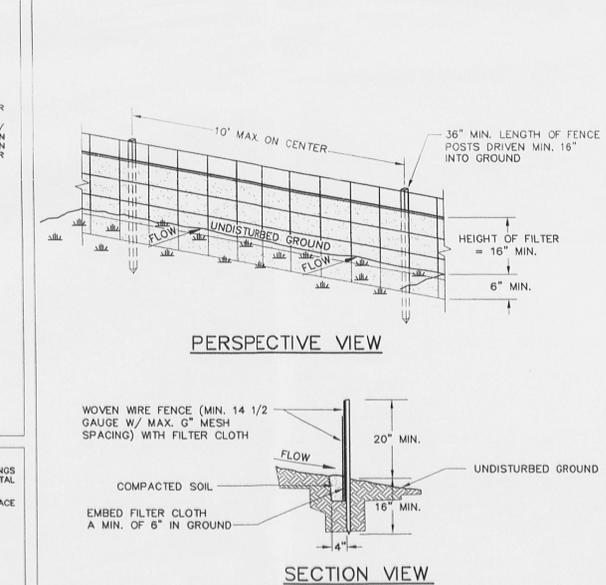
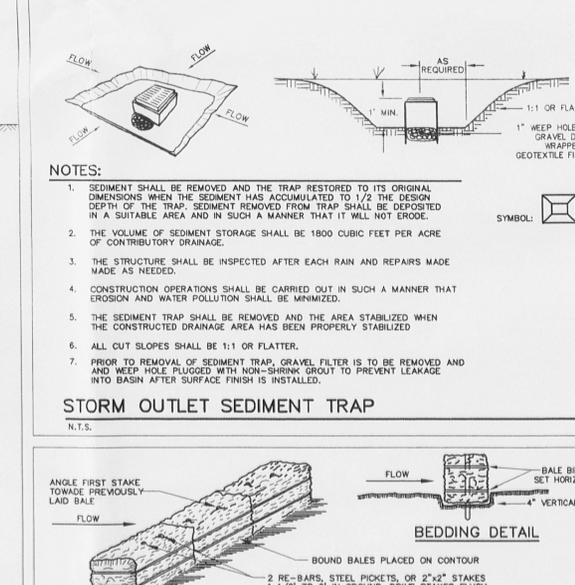
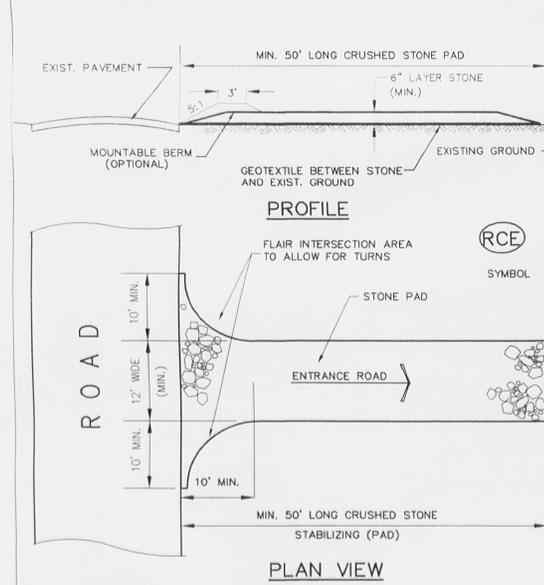
NYS DEPT. OF ENVIRONMENTAL CONSERVATION DIVISION OF WATER 825 BROADWAY, 4th FLOOR ALBANY, NY 12233-3505	CORNWALL TOWN HALL 183 MAIN STREET CORNWALL, NY 12518	TOWN OF NEW WINDSOR 555 UNION AVE. NEW WINDSOR, NY 12553
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- FOOTNOTES:**
- "COMMENCEMENT OF CONSTRUCTION" MEANS THE INITIAL REMOVAL OF VEGETATION AND DISTURBANCE OF SOILS ASSOCIATED WITH CLEARING, GRADING OR EXCAVATING ACTIVITIES OR OTHER CONSTRUCTION ACTIVITIES.
  - "FINAL STABILIZATION" MEANS THAT ALL SOIL DISTURBING ACTIVITIES AT THE SITE HAVE BEEN COMPLETED AND A LUSH, PERMANENT VEGETATIVE COVER WITH A DENSITY OF EIGHTY (80) PERCENT HAS BEEN ESTABLISHED OR EQUIVALENT STABILIZATION MEASURES SUCH AS THE USE OF MULCHES OR GEOTEXTILES HAVE BEEN EMPLOYED ON ALL UNPAVED AREAS AND AREAS NOT COVERED BY PERMANENT STRUCTURES.
  - "STABILIZATION" MEANS COVERING OR MAINTAINING AN EXISTING COVER OVER SOIL COVER CAN BE VEGETATIVE (E.G. GRASS, TREES, REEDS AND MULCH, STRUBS, OR TURF) OR NON-VEGETATIVE (E.G. GEOTEXTILES, RIPRAP OR GABIONS).
  - "SWCD" MEANS SOIL AND WATER CONSERVATION DISTRICT.
  - "SPDES" MEANS STATE POLLUTION DISCHARGE ELIMINATION SYSTEM.

## D. RESPONSIBILITY AND IMPLEMENTATION OF THE STORMWATER POLLUTION PREVENTION PLAN.

- THE DEVELOPER IS THE PERMITTEE PURSUANT TO THE NEW YORK STATE SPDES PERMIT COVERING THE WORK. THE CONTRACTOR AND HIS/HER SUBCONTRACTORS, IS THE OPERATOR OF ANY EROSION CONTROL PRACTICE INSTALLED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS, OR AS DIRECTED.
- UPON COMPLETION OF WORK, AND ACCEPTANCE OF THE STORMWATER POLLUTION PREVENTION SYSTEM CONSTRUCTED IN ACCORDANCE WITH THESE PLANS, THE PROPERTY OWNER WILL BE RESPONSIBLE FOR THE OPERATION AND MAINTENANCE OF SAID SYSTEM IN ACCORDANCE WITH THE CONDITIONS OF SAID "SPDES" PERMIT. THE PROPERTY OWNER WILL BE THE OPERATOR REFERRED TO HEREINAFTER.

## I. DRAINAGE CHARACTERISTICS AND CONTROL

- THE WORK AREA IS PRIMARILY LIMITED TO THE VICINITY OF THE THREE BUILDINGS ON THE SITE. THE DRAINAGE FROM THESE AREAS IS DIVERTED IN TWO DIRECTIONS BY THE ROOF OF BUILDING NO. 1, THE LARGEST BUILDING. RUNOFF FROM THE EAST SIDE EXITS THE PRIMARY DRAINAGE AREA THROUGH A DITCHING CHANNEL ADJACENT TO THE CHAINLINK FENCE. THE WEST SIDE RUNOFF CROSSES INTO THE ADJONING WETLAND AROUND THE NOW ABANDONED FILTER MOUND.
- SURFACE RUNOFF WATER IS RELATIVELY CLEAN AND SEDIMENT FREE AND SHALL BE DIVERTED FROM THE WORK WHENEVER POSSIBLE.



PLANNING BOARD  
TOWN OF CORNWALL  
Preliminary & Final Approval

SEP 10 2007

Chmn. *[Signature]*

RESERVED FOR APPROVAL

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THIS DRAWING IS ONE IN A SET OF DRAWINGS & IS INCOMPLETE & INVALID WHEN IT IS SEPARATED FROM THIS SET.

Unauthorized alteration or addition to this drawing is a violation of Section 7209 (2) of the NYS Education Law.

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**ES1**

OF 4

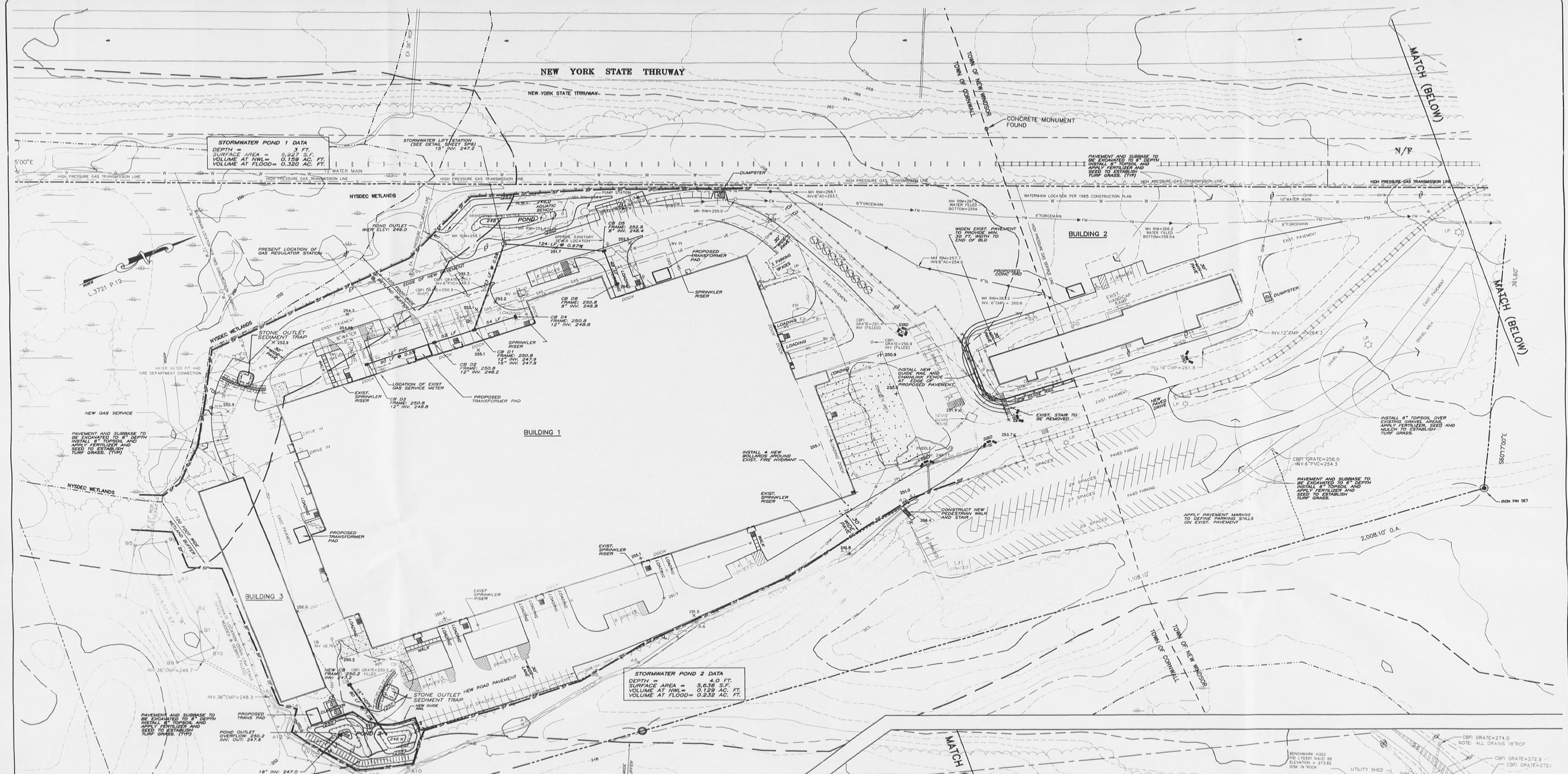
**TITLE**  
STORMWATER POLLUTION PREVENTION NOTES AND DETAILS

**JOB**  
06013

**DATE**  
SEPT. 25, 2006

**SCALE**  
AS NOTED

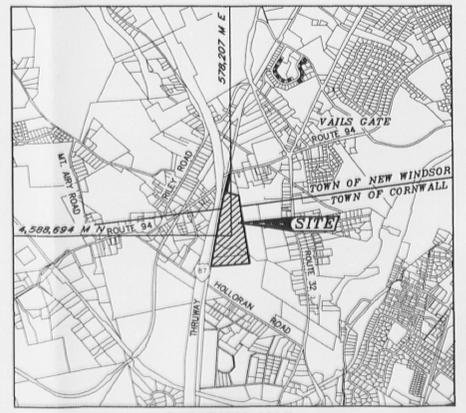
**DWG.**



**STORMWATER POND 1 DATA**  
 DEPTH = 4.0 FT.  
 SURFACE AREA = 0.927 S.F.  
 VOLUME AT NWL = 0.159 AC. FT.  
 VOLUME AT FLOOD = 0.320 AC. FT.

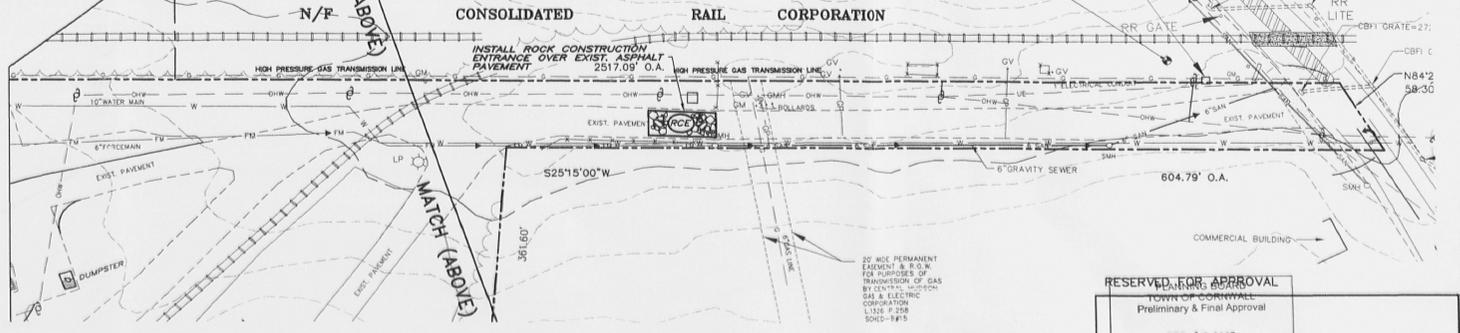
**STORMWATER POND 2 DATA**  
 DEPTH = 4.0 FT.  
 SURFACE AREA = 5.636 S.F.  
 VOLUME AT NWL = 0.129 AC. FT.  
 VOLUME AT FLOOD = 0.232 AC. FT.

LEGEND	
<b>SYMBOLS</b>	<b>ABBREVIATIONS</b>
--- PROPERTY LINE	CB CATCH BASIN
--- ADJOINING LOT LINE	CD/P PERF. SEPTIC CURTAIN DRAIN PIPE
--- EASEMENT LINE	CD/S SOLID SEPTIC CURTAIN DRAIN PIPE
--- CONTOURS	CL CENTERLINE
--- WATER EDGE OR STREAM	CMR CORRUGATED METAL PIPE
--- GUIDE RAIL	C.O. CLEAN OUT
--- FENCE LINE	CONC. CONCRETE
--- STONEWALL	D.I.P. DUCTILE IRON PIPE
--- EDGE OF PAVEMENT	ELEV. ELEVATION
--- GAS MAIN	FTE FINISHED FLOOR ELEVATION
--- OVERHEAD UTILITY LINE	FP FLAG POLE (EXIST)
--- TREELINE	GV GAS VALVE
--- ROAD SIGN	G.F.A. GROSS FLOOR AREA
--- UTILITY POLE	HDPE HIGH DENSITY POLYETHYLENE (SMOOTH INTERIOR)
--- WELL	HP HIGH POINT
--- WATERMAIN GATE VALE	INVERT
--- HYDRANT (EXIST)	LF LINEAR FOOT
--- RAILROAD TRACK	LP LOW POINT
--- DIVERSION SWALE	LSE LOWEST SEWER ELEVATION (NV)
--- AREA LIGHT POLE	MH MANHOLE
--- ROOF/FOOTING DRAINS	MW MONITORING WELL
--- WETLAND BOUNDARY LINE	N.W.L. NORMAL WATER LEVEL (SURFACE ELEV)
--- WETLAND BUFFER LINE (100 FT)	O.A. OVER ALL (DISTANCE)
--- WETLAND AREA	OE OVERHEAD ELECTRIC
--- CONCRETE SURFACES	OHW OVERHEAD WIRES
--- PROPOSED BITUMINOUS SURFACES	PROP. PROPOSED
--- BITUMINOUS SURFACES (TO BE REMOVED)	RCV REINFORCED CONCRETE PIPE
--- PROPOSED SILT FENCE	W.L. WATER LEVEL (SURFACE ELEV)
--- PROPOSED STRAW BALE DIKE	W.V. WATER VALVE
	SBD STORM OUTLET SEDIMENT TRAP
	RCE STABILIZED ROCK CONSTRUCTION ENTRANCE



**SOIL EROSION AND SEDIMENT CONTROL PLAN**

SCALE: 1" = 50'  
**NOTE:**  
 1. PERMANENT STORMWATER FACILITIES CANNOT BE UTILIZED DURING CONSTRUCTION  
 2. REFER TO DRAWING EST FOR NOTES AND DETAILS



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RESERVED FOR APPROVAL  
 TOWN OF CORNWALL  
 Primary & Final Approval  
 SEP 10 2007  
 APPROVAL GRANTED BY TOWN OF NEW WINDSOR  
 AUG 23 2007

NO.	DATE	DESCRIPTION
1	6/26/07	REVISED AS PER NYSDEC COMMENTS 6/22/07
2	7/18/07	SMP POND REVISIONS, 2 OF 4 SHEETS
3	7/12/07	ADDED INTERIM POND
4	2/20/07	REVIEW COMMENTS
5	1/29/07	SEC (WHITE PAPER) SUBMITTAL
6	12/20/06	UPDATE SET IMPROVEMENTS

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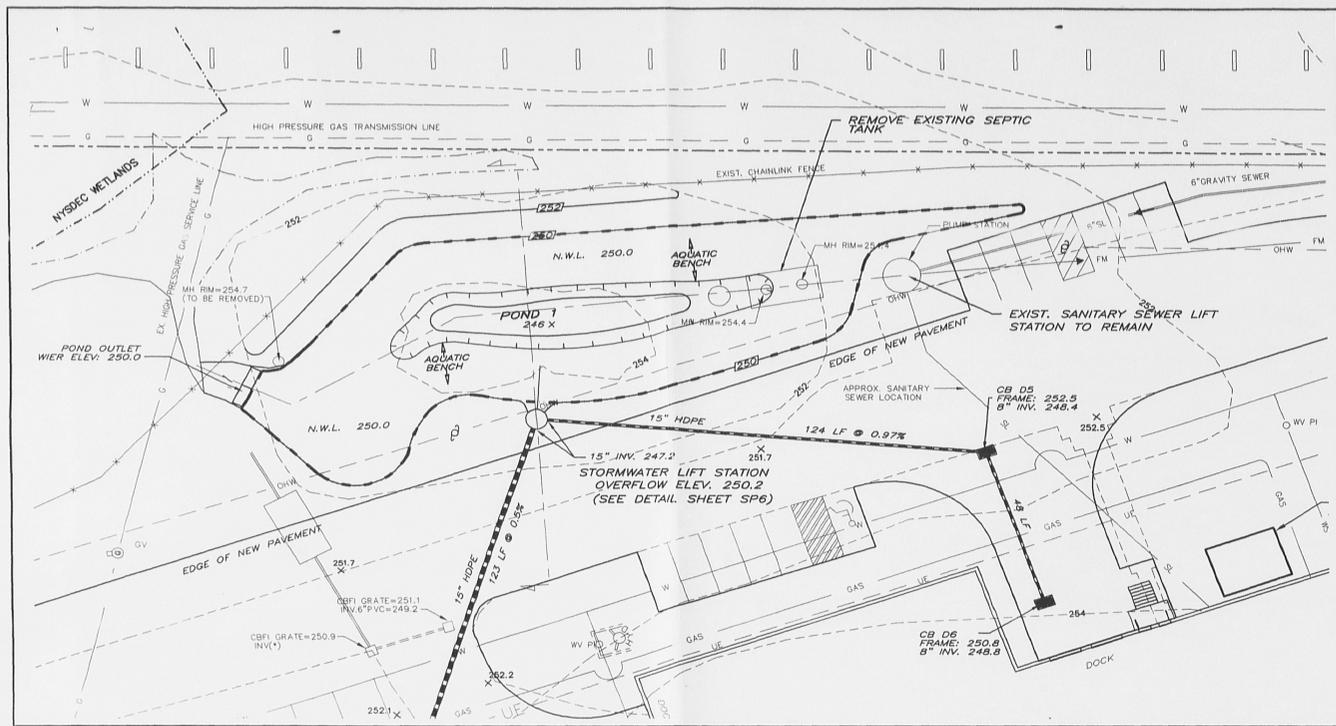
**VAILS GATE BUSINESS PARK**  
**KESSLER PROPERTIES**  
 NY ROUTE 94  
 TOWN OF NEW WINDSOR AND TOWN OF CORNWALL  
 ORANGE COUNTY, NEW YORK



**TITLE**  
 SOIL EROSION AND SEDIMENT CONTROL PLAN

JOB: 06013  
 DATE: SEPT. 25, 2006  
 SCALE: 1" = 50'  
 DWG.

**ES2**  
 OF 4



PLAN - WATER QUALITY POND 1

SCALE: 1" = 20'

KEY

- PLANT REFERENCE MARKER (SEE PLANTING SCHEDULE)
- QUANTITY OF PLANTINGS
- 'EMERGENT' ZONE (PLANT WITH SEED MIX)

PLANTING SCHEDULE

PLANT TYPE	SPRING PLANTING	FALL PLANTING
CONIF.	APRIL 1 - JUNE 30	SEPT. 1 - OCT. 15
DEC.	MARCH 1 - JUNE 30	OCT. 1 - DEC. 1
HERBACEOUS	MARCH 1 - JUNE 30	SEPT 1 - OCT. 15

POCKET POND PLANTING NOTES

- TERM "LANDSCAPER" SHALL MEAN CONTRACTOR THAT FURNISHES, INSTALLS AND WARRANTS THE PLANT MATERIALS INDICATED ON THE PLANS. THE LANDSCAPER SHALL PROVIDE NEW PLANT MATERIALS THAT ARE DELIVERED TO THE SITE IN HEALTHY CONDITION, ADEQUATELY WATERED AND PROPERLY CARED FOR PRIOR TO INSTALLATION. THE LANDSCAPER SHALL INSTALL PLANT MATERIALS AS SOON AS POSSIBLE AFTER DELIVERY TO THE SITE, AND PROVIDE NECESSARY CARE AFTER INSTALLATION TO PROMOTE HEALTHY, VIGOROUS GROWTH.
- NEW PLANT MATERIAL SHALL BE OBTAINED FROM CERTIFIED NURSERIES AND CONFORM TO THE STANDARDS OF "AMERICAN NURSERY STOCK", PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERMEN. PLANT MATERIAL SHALL BE WARRANTED FOR ONE (1) YEAR AFTER FINAL ACCEPTANCE OF PROJECT PLANTINGS. DEAD, DYING OR PLANT MATERIAL THAT, IN THE OPINION OF THE ENGINEER, IS UNHEALTHY OR IN POOR CONDITION, SHALL BE REPLACED AT NO ADDITIONAL COST. REPLACEMENT OF PLANT MATERIAL SHALL BE DONE AS SOON AS POSSIBLE AFTER THE PLANT IS CERTIFIED AS UNACCEPTABLE, BUT NO LATER THAN THE NEXT GROWING SEASON.
- THE LOCATION OF PLANT MATERIAL TO BE INSTALLED SHALL BE STAKED OUT BY THE LANDSCAPER, AND REVIEWED BY THE ENGINEER PRIOR TO COMMENCING PLANTINGS. THE LOCATION OF UNDERGROUND UTILITIES AND OBSTRUCTIONS MAY REQUIRE THE SITE OF SOME PLANTS TO MOVE FROM THE POSITION SHOWN ON THE PLANS.
- THE LANDSCAPER SHALL PREPARE THE SURFACE OF AREAS TO RECEIVE PLANT MATERIALS IN ACCORDANCE WITH THE PLANTING DETAILS AND SPECIFICATIONS, AND/OR AS REQUIRED BY THE PARTICULAR PLANT TYPE AND LOCATION (I.E. UNDERWATER, EMERGENT FLOOD ZONE, SIDE SLOPES, ETC.) SURFACES SHALL BE LOOSELY COMPACTED SOIL, FREE OF STUMPS, ROOTS AND WOODY MATERIAL, AND STONES LARGER THAN ONE INCH (1") IN SIZE.
- THE SOIL IN AND AROUND THE STORMWATER MANAGEMENT PRACTICE (SMP) SHOWN HEREON WILL HAVE BEEN COMPACTED DURING CONSTRUCTION TO INSURE STABILITY. THE DENSITY OF THE COMPACTED SOIL IS UNSUITABLE FOR PLANT ROOT PENETRATION. THE LANDSCAPER SHALL EXCAVATE PLANTING HOLES THREE TO FIVE TIMES WIDER AND DEEPER THAN THE SIZE OF THE ROOT BALL OR CONTAINER OF THE PLANT STOCK, AND BACKFILL WITH LOOSE, UNCOMPACTED SOIL.
- PLANT MATERIAL DELIVERED IN CONTAINERS (E.G. CANS OR POTS), PLASTIC OR NON-ROT BURLAP SHALL BE REMOVED FROM THE CONTAINER, AND THEIR ROOT BALLS ARE TO BE SCARIFIED PRIOR TO BACKFILLING. PLANT MATERIALS SHALL BE WATERED ONCE PER DAY FOR FOURTEEN (14) CONSECUTIVE DAYS FOLLOWING INSTALLATION.
- THE LANDSCAPER SHALL MAINTAIN THE PLANT MATERIALS IN HEALTHY, VIGOROUS GROWING CONDITION THROUGHOUT THE DURATION OF THE WARRANTY PERIOD. THE INSTALLATION SHALL BE REGULARLY INSPECTED BY THE LANDSCAPER TO MAXIMIZE THE SURVIVAL OF THE VEGETATION (7% OF HERBACEOUS PLANTINGS, 100% OF TREES AND SHRUBS).
- MULCH: UNLESS DIRECTED OTHERWISE, WOOD MULCH SHALL BE APPLIED TO SURFACE OF POND PERIMETER THAT HAS BEEN PLANTED, TO A DEPTH NOT TO EXCEED THREE INCHES (3"). WOOD MULCH SHALL BE STANDARD LANDSCAPE STYLE, SHREDED HARDWOOD MULCH, AGED FOR ONE YEAR AND FREE OF OTHER MATERIALS (ROOTS, SOIL, WEEDS, ROOTS, ETC.).
- REFER TO PLANTING SCHEDULE, PLANTING DETAIL AND GROUND COVER SCHEDULE FOR LIST OF PLANT MATERIALS AND INSTALLATION REQUIREMENTS. THE SCHEDULE OF PLANTING LISTS THE APPROPRIATE TIME OF YEAR THAT PLANT MATERIALS SHOULD BE INSTALLED.
- UPON COMPLETION OF THE WARRANTY PERIOD, PLANT MATERIAL SHALL HAVE ROOTED WELL, AND APPEAR TO BE STRONG WITH LUSH, NEW GROWTH. DEAD, SICK OR DAMAGED PLANTS SHALL BE REPLACED IN ACCORDANCE WITH THE PRECEDING.

POND 1 OUTLET STRUCTURE

SCALE: 1/2" = 1'

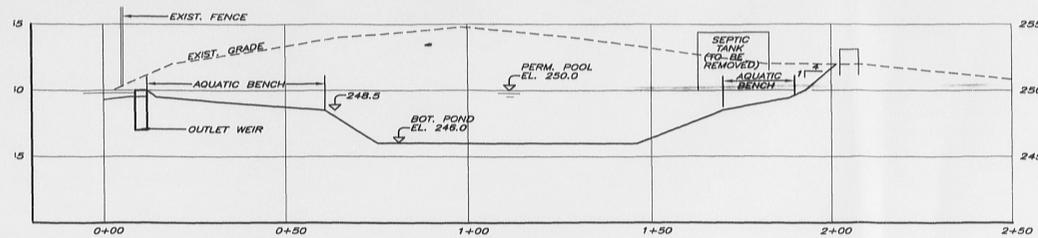
POND/WETLAND PLANTING SCHEDULE

SYMBOL	QTY	COMMON NAME	FORM	SIZE/CALIPER	REMARKS
TREES AND SHRUBS					
DS	36	SILKY DOGWOOD (CORNUS AMOMIUM)	DEC. SHRUB	GAL. CONT.	SPACE AT 30" OC
PO	3	PIN OAK (QUERCUS PALAISTRIS)	DEC. TREE	2"-2 1/2" CAL.	BALLED AND BURLAP (B & B)
RC	9	EASTERN RED CEDAR (JUNIPERUS VIRGINIANA)	CONF. TREE	4"-5 1/2" HGT.	B & B
RM	3	RED MAPLE (ACER RUBRUM)	DEC. TREE	2"-2 1/2" CAL.	B & B

SYMBOL	QTY	COMMON NAME	FORM	REMARKS
HERBACEOUS PLANTS				
BH	36	HARDSTEM BULRUSH (SCIRPUS ACUTUS)	EMERGENT	BARE ROOT (BR)
DU	36	DUCKWEED (LEMNA SP.)	SUBMERGENT/EMERGENT	BR
PW	55	LONG-LEAVED POND WEED (POTAMOGETON NODOSUS)	ROOTED SUBMERGENT/AQUATIC	BR
SD	25	SPATTERDOCK (NUPHAR LUTEUM)	EMERGENT	BR
WC	42	WILD CELERY (VALSNERIA AMERICANA)	SUBMERGENT	BR

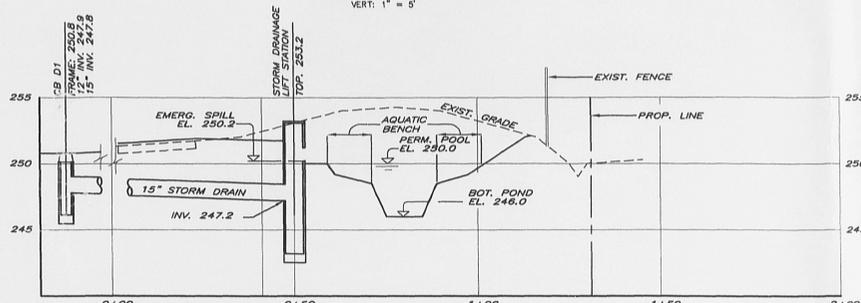
PERCENT BY # OF SEEDS	COMMON NAME	SCIENTIFIC NAME	REMARKS
63.0 %	CREEPING BENTGRASS	ARGROSTIS STOLONIFERA	"NORTHEAST WETLAND DIVERSITY SEED MIX" AVAILABLE FROM : SOUTHERN TIER CONSULTING WEST CLARKSVILLE, NY 14788
17.0 %	ROUGH BLUEGRASS	POA TRIVIALIS	APPLIED TO POND EMERGENT ZONE (AREA BETWEEN POND NORMAL WATER LEVEL AND HIGH WATER LEVEL INDICATED ON PLANS.)
11.0 %	MEADOW FOXTAIL	ALOPECURUS ARUNDINACEUS	
4.5 %	ANNUAL RYEGRASS	LOLIUM MULTIFLORUM	SEED MIX TO BE APPLIED AT 1 LB PER POND (OR 2 1/2 LBS PER ACRE BASED ON AREA)
4.5 %	DEERTONGUE	PANICUM CLANDESTINUM	

BR - BARE ROOT CS - CLUMPS OF 5 OC - ON CENTER



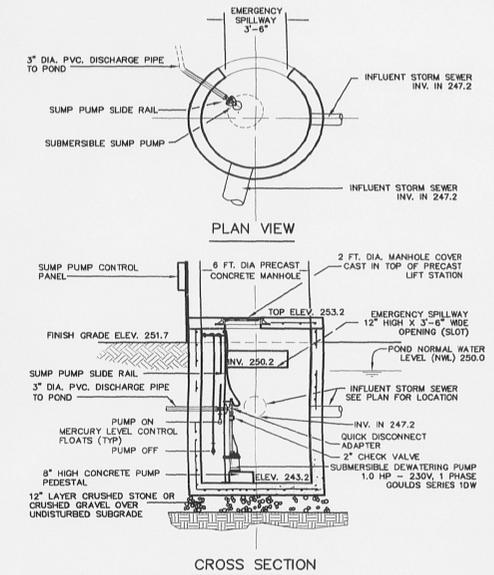
PROFILE - POND 1

SCALE: HORIZ: 1" = 20' VERT: 1" = 5'



CROSS SECTION - POND 1

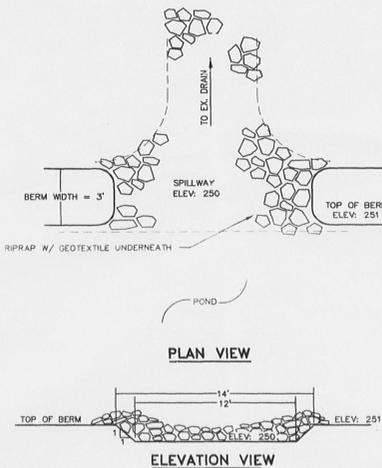
SCALE: HORIZ: 1" = 20' VERT: 1" = 5'



STORM DRAINAGE LIFT STATION

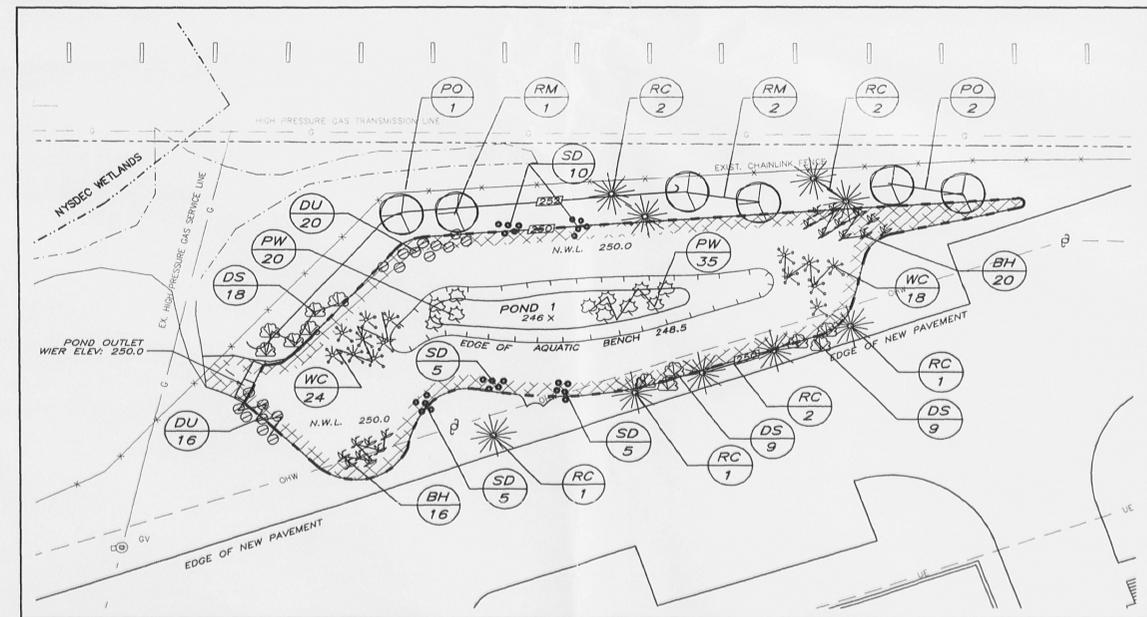
SCALE: 1/4" = 1'

STORMWATER POND 1 DATA  
 DEPTH = 3 FT.  
 SURFACE AREA = 6,927 S.F.  
 VOLUME AT NWL = 0.159 AC. FT.  
 VOLUME AT FLOOD = 0.320 AC. FT.



PLAN VIEW

ELEVATION VIEW



LANDSCAPE PLAN - POND 1

SCALE: 1" = 20'

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RESERVED FOR APPROVAL  
 TOWN OF CORNWALL  
 Preliminary & Final Approval  
 SEP 10 2007  
 Glyn M. DeLong

THIS DRAWING IS ONE IN A SET OF DRAWINGS & IS INCOMPLETE & INVALID WHEN IT IS SEPARATED FROM THIS SET.  
 Unauthorized alteration or addition to this drawing is a violation of Section 7209 (2) of the NYS Education Law.

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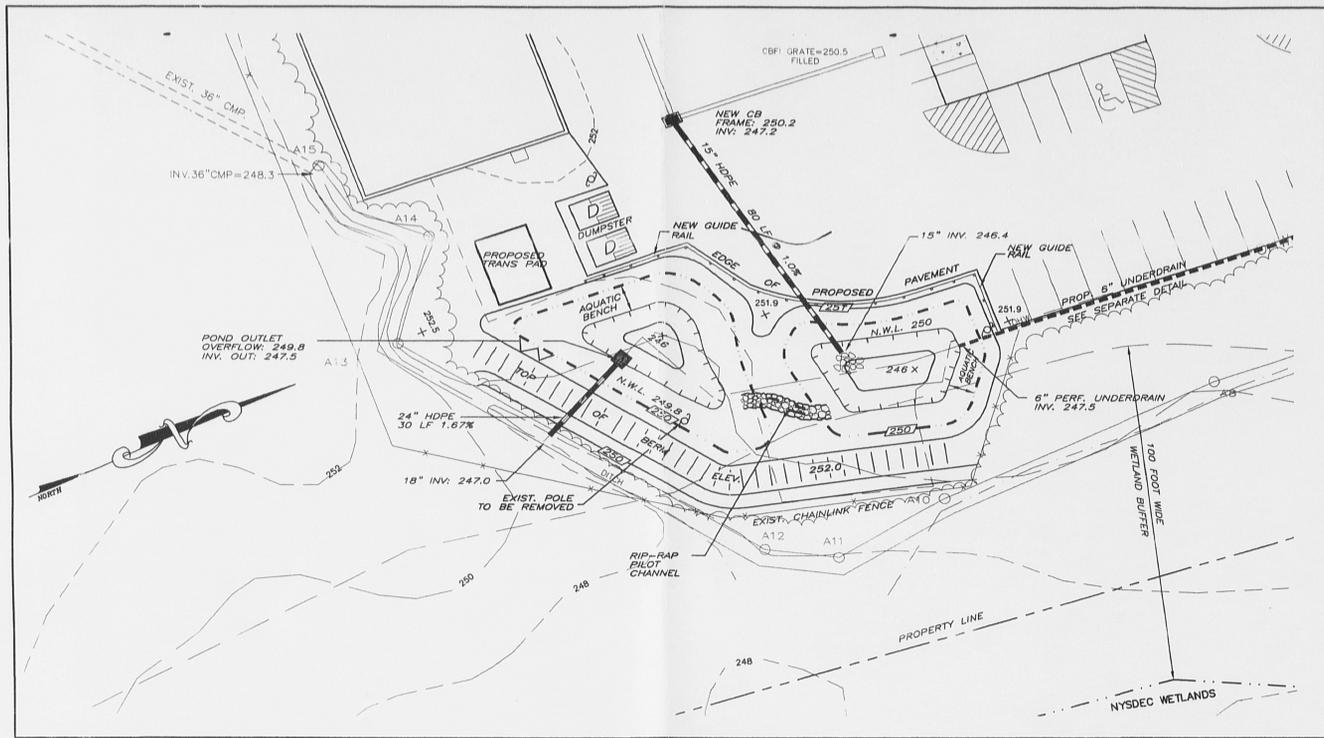
VAILE GATE BUSINESS PARK  
 KESSLER PROPERTIES  
 NY ROUTE 94  
 TOWN OF NEW WINDSOR AND TOWN OF CORNWALL  
 ORANGE COUNTY, NEW YORK



TITLE WATER QUALITY POND 1 PLAN AND LANDSCAPING

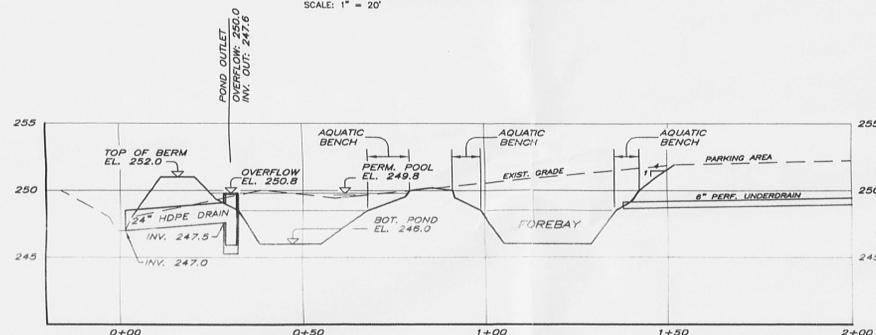
JOB 06013  
 DATE APRIL 18, 2007  
 SCALE AS NOTED

ES3



PLAN - WATER QUALITY POND 2

SCALE: 1" = 20'



PROFILE - POND 2

SCALE: HORIZ: 1" = 20'  
VERT: 1" = 5'

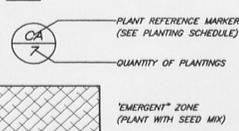
**STORMWATER POND 2 DATA**

DEPTH	=	4.0 FT.
SURFACE AREA	=	5,936 S.F.
VOLUME AT NWL	=	0.129 AC. FT.
VOLUME AT FLOOD	=	0.232 AC. FT.

**POCKET POND PLANTING NOTES**

1. TERM "LANDSCAPER" SHALL MEAN CONTRACTOR THAT FURNISHES, INSTALLS AND WARRANTS THE PLANT MATERIALS INDICATED ON THE PLANS. THE LANDSCAPER SHALL PROVIDE NEW PLANT MATERIALS THAT ARE DELIVERED TO THE SITE IN HEALTHY CONDITION, ADEQUATELY WATERED AND PROPERLY CARED FOR PRIOR TO INSTALLATION. THE LANDSCAPER SHALL INSTALL PLANT MATERIALS AS SOON AS POSSIBLE AFTER DELIVERY TO THE SITE, AND PROVIDE NECESSARY CARE AFTER INSTALLATION TO PROMOTE HEALTHY, VIGOROUS GROWTH.
2. NEW PLANT MATERIAL SHALL BE OBTAINED FROM CERTIFIED NURSERIES, AND CONFORM TO THE STANDARDS OF "AMERICAN NURSERY STOCK", PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERMEN. PLANT MATERIAL SHALL BE WARRANTED FOR ONE (1) YEAR AFTER FINAL ACCEPTANCE OF PROJECT PLANTINGS. DEAD, DYING OR PLANT MATERIAL THAT, IN THE OPINION OF THE ENGINEER, IS UNHEALTHY OR IN POOR CONDITION SHALL BE REPLACED AT NO ADDITIONAL COST. REPLACEMENT OF PLANT MATERIAL SHALL BE DONE AS SOON AS POSSIBLE AFTER THE PLANT IS CERTIFIED AS UNACCEPTABLE, BUT NO LATER THAN THE NEXT GROWING SEASON.
3. THE LOCATION OF PLANT MATERIAL TO BE INSTALLED SHALL BE STAKED OUT BY THE LANDSCAPER, AND REVIEWED BY THE ENGINEER PRIOR TO COMMENCING PLANTINGS. THE LOCATION OF UNDERGROUND UTILITIES AND OBSTRUCTIONS MAY REQUIRE THE SITE OF SOME PLANTS TO MOVE FROM THE POSITION SHOWN ON THE PLANS.
4. THE LANDSCAPER SHALL PREPARE THE SURFACE OF AREAS TO RECEIVE PLANT MATERIALS IN ACCORDANCE WITH THE PLANTING DETAILS AND SPECIFICATIONS, AND/OR AS REQUIRED BY THE PARTICULAR PLANT TYPE AND LOCATION (I.E., UNDERWATER, EMERGENT FLOOD ZONE, SIDE SLOPES, ETC.) SURFACES SHALL BE LOOSELY COMPACTED SOIL, FREE OF STUMPS, ROOTS AND WOODY MATERIAL, AND STONES LARGER THAN ONE INCH (1") IN SIZE.
5. THE SOIL IN AND AROUND THE STORMWATER MANAGEMENT PRACTICE (SMP) SHOWN HEREON WILL HAVE BEEN COMPACTED DURING CONSTRUCTION TO INSURE STABILITY. THE DENSITY OF THE COMPACTED SOIL IS UNSUITABLE FOR PLANT ROOT PENETRATION. THE LANDSCAPER SHALL EXCAVATE PLANTING HOLES THREE TO FIVE TIMES WIDER AND DEEPER THAN THE SIZE OF THE ROOT BALL OR CONTAINER OF THE PLANT STOCK, AND BACKFILL WITH LOOSE, UNCOMPACTED SOIL.
6. PLANT MATERIAL DELIVERED IN CONTAINERS (E.G., CANS OR POTS), PLASTIC OR NON-ROT BURLAP SHALL BE REMOVED FROM THE CONTAINER, AND THEIR ROOT BALLS ARE TO BE SCARIFIED PRIOR TO BACKFILLING. PLANT MATERIALS SHALL BE WATERED ONCE PER DAY FOR FOURTEEN (14) CONSECUTIVE DAYS FOLLOWING INSTALLATION.
7. THE LANDSCAPER SHALL MAINTAIN THE PLANT MATERIALS IN HEALTHY, VIGOROUS GROWING CONDITION THROUGHOUT THE DURATION OF THE WARRANTY PERIOD. THE INSTALLATION SHALL BE REGULARLY INSPECTED BY THE LANDSCAPER TO MAXIMIZE THE SURVIVAL OF THE VEGETATION (7% OF HERBACEOUS PLANTINGS, 100% OF TREES AND SHRUBS).
8. MULCH: UNLESS DIRECTED OTHERWISE, WOOD MULCH SHALL BE APPLIED TO SURFACE OF POND PERIMETER THAT HAS BEEN PLANTED, TO A DEPTH NOT TO EXCEED THREE INCHES (3"). WOOD MULCH SHALL BE STANDARD LANDSCAPE STYLE, SPREADD HARDWOOD MULCH, ACED FOR ONE YEAR AND FREE OF OTHER MATERIALS (ROCKS, SOIL, WEEDS, ROOTS, ETC.).
9. REFER TO PLANTING SCHEDULE, PLANTING DETAIL, AND GROUND COVER SCHEDULE FOR LIST OF PLANT MATERIALS AND INSTALLATION REQUIREMENTS. THE SCHEDULE OF PLANTING LISTS THE APPROPRIATE TIME OF YEAR THAT PLANT MATERIALS SHOULD BE INSTALLED.
10. UPON COMPLETION OF THE WARRANTY PERIOD, PLANT MATERIAL SHALL HAVE ROOTED WELL, AND APPEAR TO BE STRONG WITH LUSH, NEW GROWTH. DEAD, SICK OR DAMAGED PLANTS SHALL BE REPLACED IN ACCORDANCE WITH THE PRECEDING.

**KEY**



**PLANTING SCHEDULE**

PLANT TYPE	SPRING PLANTING	FALL PLANTING
CONIF.	APRIL 1 - JUNE 30	SEPT. 1 - OCT. 15
DEC.	MARCH 1 - JUNE 30	OCT. 1 - DEC. 1
HERBACEOUS	MARCH 1 - JUNE 30	SEPT 1 - OCT. 15

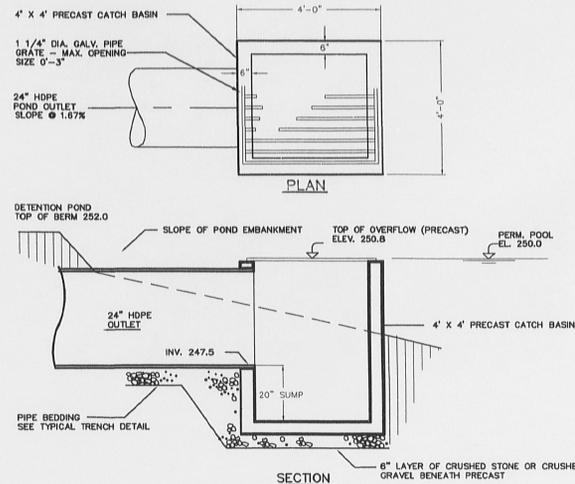
**POND/WETLAND PLANTING SCHEDULE**

SYMBOL	QTY	COMMON NAME	FORM	SIZE/CALIPER	REMARKS
TREES AND SHRUBS					
DS	33	SILKY DOGWOOD (CORNUS AMOMIUM)	DEC. SHRUB	GAL. CONT.	SPACE AT 30' OC
HERBACEOUS PLANTS					
BH	22	HARDSTEM BULRUSH (SCIRPUS ACUTUS)	EMERGENT	BARE ROOT (BR)	
DU	48	DUCKWEED (LEMNA SP.)	SUBMERGENT/EMERGENT	BR	
PW	40	LONG-LEAVED POND WEED (POTAMOGETON NODOSUS)	ROOTED SUBMERGENT AQUATIC	BR	
SD	25	SPATTERDOCK (NUPHAR LUTEUM)	EMERGENT	BR	CLUSTERS OF FIVE (CS) SPACED AT 20' O.C.
WC	38	WILD CELERY (VALISNERIA AMERICANA)	SUBMERGENT	BR	

**SEED**

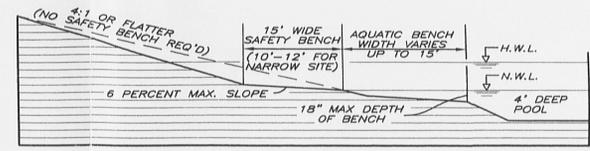
PERCENT BY # OF SEEDS	COMMON NAME	SCIENTIFIC NAME	REMARKS
83.0 %	CREeping BENTGRASS	ARGROSTIS STOLONIFERS	"NORTHEAST WETLAND DIVERSITY SEED MIX" AVAILABLE FROM: SOUTHERN TERR CONSULTING WEST CLARKSVILLE, NY 14786
17.0 %	ROUGH BLUEGRASS	POA TRIVIALIS	APPLIED TO POND EMERGENT ZONE (AREA BETWEEN POND NORMAL WATER LEVEL AND HIGH WATER LEVEL INDICATED ON PLANS.)
11.0 %	MEADOW FOXTAIL	ALOPECURUS ARUNDINACEUS	
4.5 %	ANNUAL RYEGRASS	LOLIUM MULTIFLORUM	SEED MIX TO BE APPLIED AT 1 LB PER POND (OR 2 1/2 LBS PER ACRE BASED ON AREA)
4.5 %	DEERTONGUE	PANICUM CLAUDEANUM	

BR - BARE ROOT CS - CLUMPS OF 5 OC - ON CENTER



POND 2 OUTLET STRUCTURE

SCALE: 1/2" = 1'



TYPICAL POND SECTION

SCALE: NONE



LANDSCAPE PLAN - POND 2

SCALE: 1" = 20'

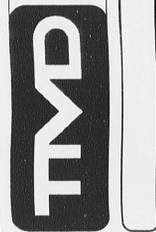
PLANNING BOARD  
TOWN OF CORNWALL  
Preliminary & Final Approval  
SEP 10 2007  
Approval granted by TOWN OF NEW WINDSOR  
AUG 23 2007  
RESERVED FOR APPROVAL

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NO.	DATE	DESCRIPTION	REVISIONS
1	6/26/07	REVISED AS PER NYSECC COMMENTS 5/16/07	BAD APPROVED

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ORANGE COUNTY, NEW YORK



TITLE **WATER QUALITY POND 2 PLAN AND LANDSCAPING**

JOB 06013  
DATE APRIL 18, 2007  
SCALE AS NOTED  
DWG.

**ES4**  
OF 4